

Ref : 216077REM

Address: The Straight, Southall Middlesex UB1 1QX

Ward: Southall Broadway

Proposal: Application for the approval of Reserved Matters pursuant to outline planning permission reference 171562VAR dated 16/10/2017, Condition 2 pertaining to the second phase of development (Phase 2): details relating to Layout, Appearance, Scale and Landscaping.

Drawing numbers:

Site Location Plan (Ref: A725-FOR-00-A-DR-10000 – Rev P2); Existing Site Plan (Ref: A725-FOR-00-A-DR-11000 – Rev P2); Site Plan, Ground Floor Plan (Ref: A725-FOR-00-A-DR-18000 – Rev P9); Site Plan, 1st Floor Plan (Ref: A725-FOR-01-A-DR-18001 – Rev P5); Site Plan, 2nd Floor Plan (Ref: A725-FOR-02-A-DR-18002 – Rev P2); Site Plan, 3rd Floor Plan (Ref: A725-FOR-03-A-DR-18003 – Rev P2); Site Plan, 4th Floor Plan (Ref: A725-FOR-04-A-DR-18004 – Rev P2); Site Plan, 5th Floor Plan (Ref: A725-FOR-05-A-DR-18005 – Rev P2); Site Plan, 6th Floor Plan (Ref: A725-FOR-06-A-DR-18006 – Rev P2); Site Plan, 7th Floor Plan (Ref: A725-FOR-07-A-DR-18007 – Rev P2); Site Plan, 8th Floor Plan (Ref: A725-FOR-08-A-DR-18008 – Rev P2); Site Plan, 9th Floor Plan (Ref: A725-FOR-09-A-DR-18009 – Rev P2); Site Plan, 10th Floor Plan (Ref: A725-FOR-10-A-DR-18010 – Rev P2); Site Plan, 11th Floor Plan (Ref: A725-FOR-11-A-DR-18011 – Rev P2); Site Plan, 12th Floor Plan (Ref: A725-FOR-12-A-DR-18012 – Rev P2); Site Plan, 13th Floor Plan (Ref: A725-FOR-13-A-DR-18013 – Rev P2); Site Plan, 17th Floor Plan (Ref: A725-FOR-17-A-DR-18017 – Rev P2); Site Plan, 18th Floor Plan (Ref: A725-FOR-18-A-DR-18018 – Rev P2); Site Plan, Roof Plan (Ref: A725-FOR-RF-A-DR-18100 – Rev P9); Site Plan, Basement Plan (Ref: A725-FOR-B1-A-DR-18099 – Rev P3); Block A, Ground Floor Plan (Ref: A738-FOR-00-A-DR-12000 – Rev P6); Block A, 1st-16th Floor Plan (Ref: A738-FOR-01-A-DR-12001 – Rev P6); Block A, 17th Floor Plan (Ref: A738-FOR-17-A-DR-12017 – Rev P6); Block A, Roof Plan (Ref: A738-FOR-RF-A-DR-12019 – Rev P2); Block E, Ground Floor Plan (Ref: A742-FOR-00-A-DR-12000 – Rev P7); Block E, 1st, 3rd, 5th, 7th Floor Plan (Ref: A742-FOR-01-A-DR-12001 – Rev P6); Block E, 2nd, 4th, 6th Floor Plan (Ref: A742-FOR-02-A-DR-12002 – Rev P3); Block E, 8th Floor Plan (Ref: A742-FOR-08-A-DR-12008 – Rev P5); Block E, Roof Plan (Ref: A742-FOR-RF-A-DR-12009 – Rev P2); Block F, Ground Floor Plan (Ref: A743-FOR-00-A-DR-12000 – Rev P8); Block F, 1st Floor Plan (Ref: A743-FOR-01-A-DR-12001 – Rev P6); Block F, 2nd-6th Floor Plan (Ref: A743-FOR-02-A-DR-12002 – Rev P5); Block F, Roof Plan (Ref: A743-FOR-RF-A-DR-12007 – Rev P2); Block G, Ground Floor Plan (Ref: A744-FOR-00-A-DR-12000 – Rev P6); Block G, 1st Floor Plan (Ref: A744-FOR-01-A-DR-12001 – Rev P6); Block G, 2nd-6th Floor Plan (Ref: A744-FOR-02-A-DR-12002 – Rev P2); Block G, Roof Plan (Ref: A744-FOR-RF-A-DR-12007 – Rev P2); Block H, Ground Floor Plan (Ref: A765-FOR-00-A-DR-12000 – Rev P6); Block H, 1st Floor Plan (Ref: A765-FOR-01-A-DR-12001 – Rev P6); Block H, 2nd-6th Floor Plan (Ref: A765-FOR-02-A-DR-12002 – Rev P2); Block H, Roof Plan (Ref: A765-FOR-RF-A-DR-12007 – Rev P2); Block I, Ground Floor Plan (Ref: A766-FOR-00-A-DR-12000 – Rev P6); Block I, 1st Floor Plan (Ref: A766-FOR-01-A-DR-12001 – Rev P7); Block I, 2nd to 6th Floor Plan (Ref: A766-FOR-02-A-DR-12002 – Rev P3); Block I, 7th to 9th Floor Plan (Ref: A766-FOR-07-A-DR-12007 – Rev P5); Block I, 10th-11th Floor Plan (Ref: A766-FOR-10-A-DR-12010 – Rev P5); Block I, Roof Plan (Ref: A766-FOR-RF-A-DR-12012 – Rev P2); Block J, K & L, Ground Floor Plan (Ref: A745-FOR-00-A-DR-12000 – Rev P6); Block J & K 1st Floor Plan (Ref: A745-FOR-01-A-DR-12001 – Rev P8); Block J & K 2nd to 5th Floor Plan (Ref: A745-FOR-02-A-DR-12002 – Rev P2); Block J & K, 6th Floor Plan (Ref: A745-FOR-06-A-DR-12006 – Rev P7); Block J & K, 7th Floor Plan (Ref: A745-FOR-07-A-DR-12007 – Rev P6); Block J & K,

Roof Plan (Ref: A745-FOR-RF-A-DR-12008 – Rev P2); Block L, 1st Floor Plan (Ref: A767-FOR-01-A-DR-12001 – Rev P5); Block L, 2nd-9th Floor Plan (Ref: A767-FOR-02-A-DR-12002 – Rev P2); Block L, 10th-11th Floor Plan (Ref: A767-FOR-10-A-DR-12010 – Rev P5); Block L, Roof Plan (Ref: A767-FOR-RF-A-DR-12012 – Rev P2); Block M, Ground Floor Plan (Ref: A746-FOR-00-A-DR-12000 – Rev P5); Block M, 01st-06th Floor Plan (Ref: A746-FOR-01-A-DR-12001 – Rev P5); Block M, Roof Plan (Ref: A746-FOR-RF-A-DR-12007 – Rev P2); Block N, Ground Floor Plan (Ref: A757-FOR-00-A-DR-12000 – Rev P5); Block N, 01st, 3rd, 5th, 7th & 9th Floor Plan (Ref: A757-FOR-01-A-DR-12001 – Rev P5); Block N, 2nd, 4th, 6th & 8th Floor Plan (Ref: A757-FOR-02-A-DR-12002 – Rev P3); Block N, 10th Floor Plan (Ref: A757-FOR-10-A-DR-12010 – Rev P5); Block N, Roof Plan (Ref: A757-FOR-RF-A-DR-12012 – Rev P2); Block O, Ground Floor Plan (Ref: A758-FOR-00-A-DR-12000 – Rev P6); Block O, 1st-6th Floor Plan (Ref: A758-FOR-01-A-DR-12001 – Rev P5); Block O, 7th Floor Plan (Ref: A758-FOR-07-A-DR-12007 – Rev P5); Block O, 8th Floor Plan (Ref: A758-FOR-08-A-DR-12008 – Rev P5); Block O, Roof Plan (Ref: A758-FOR-RF-A-DR-12009 – Rev P2); Block P, Ground Floor Plan (Ref: A759-FOR-00-A-DR-12000 – Rev P6); Block P, 1st-6th Floor Plan (Ref: A759-FOR-01-A-DR-12001 – Rev P5); Block P, 7th-9th Floor Plan (Ref: A759-FOR-07-A-DR-12007 – Rev P5); Block P, 10th-11th Floor Plan (Ref: A759-FOR-10-A-DR-12010 – Rev P5); Block P, Roof Plan (Ref: A759-FOR-RF-A-DR-12012 – Rev P2); Block Q, Ground Floor Plan (Ref: A747-FOR-00-A-DR-12000 – Rev P5); Block Q, 1st-9th Floor Plan (Ref: A747-FOR-01-A-DR-12001 – Rev P5); Block Q, 10th Floor Plan (Ref: A747-FOR-10-A-DR-12010 – Rev P5); Block Q, Roof Plan (Ref: A747-FOR-RF-A-DR-12011 – Rev P2); Block R, Ground Floor Plan (Ref: A760-FOR-00-A-DR-12000 – Rev P5); Block R, 1st-6th Floor Plan (Ref: A760-FOR-01-A-DR-12001 – Rev P5); Block R, 7th-9th Floor Plan (Ref: A760-FOR-07-A-DR-12007 – Rev P6); Block R, 10th Floor Plan (Ref: A760-FOR-10-A-DR-12010 – Rev P6); Block R, Roof Plan (Ref: A760-FOR-RF-A-DR-12011 – Rev P2); Block S, Ground Floor Plan (Ref: A762-FOR-00-A-DR-12000 – Rev P6); Block S, 1st-6th Floor Plan (Ref: A762-FOR-01-A-DR-12001 – Rev P5); Block S, 7th Floor Plan (Ref: A762-FOR-07-A-DR-12007 – Rev P5); Block S, 8th Floor Plan (Ref: A762-FOR-08-A-DR-12008 – Rev P5); Block S, Roof Plan (Ref: A762-FOR-RF-A-DR-12009 – Rev P2); Block T & U, Ground Floor Plan (Ref: A748-FOR-00-A-DR-12000 – Rev P6); Block T & U, 1st-5th Floor Plan (Ref: A748-FOR-01-A-DR-12001 – Rev P5); Block T & U, 6th-10th Floor Plan (Ref: A748-FOR-06-A-DR-12006 – Rev P5); Block T & U, 11th-12th Floor Plan (Ref: A748-FOR-11-A-DR-12011 – Rev P5); Block T & U, Roof Plan (Ref: A748-FOR-RF-A-DR-12013 – Rev P2); Block A, Elevation 01 (Ref: A738-FOR-EV-A-DR-20000 – Rev P7); Block A, Elevation 02 (Ref: A738-FOR-EV-A-DR-20001 – Rev P7); Block A, Elevation 03 (Ref: A738-FOR-EV-A-DR-20002 – Rev P7); Block A, Elevation 04 (Ref: A738-FOR-EV-A-DR-20003 – Rev P7); Block E, Elevation 01 (Ref: A742-FOR-EV-A-DR-20000 – Rev P4); Block E, Elevation 02 (Ref: A742-FOR-EV-A-DR-20001 – Rev P4); Block E, Elevation 03 & 04 (Ref: A742-FOR-EV-A-DR-20002 – Rev P4); Block F, Elevation 01 (Ref: A743-FOR-EV-A-DR-20000 – Rev P6); Block F, Elevation 02 (Ref: A743-FOR-EV-A-DR-20001 – Rev P5); Block F, Elevation 03 & 04 (Ref: A743-FOR-EV-A-DR-20002 – Rev P5); Block G, Elevation 01 (Ref: A744-FOR-EV-A-DR-20000 – Rev P4); Block G, Elevation 02 (Ref: A744-FOR-EV-A-DR-20001 – Rev P4); Block G, Elevation 03 & 04 (Ref: A744-FOR-EV-A-DR-20002 – Rev P4); Block H, Elevation 01 (Ref: A765-FOR-EV-A-DR-20000 – Rev P4); Block H, Elevation 02 (Ref: A765-FOR-EV-A-DR-20001 – Rev P4); Block H, Elevation 03 & 04 (Ref: A765-FOR-EV-A-DR-20002 – Rev P4); Block I, Elevation 01 (Ref: A766-FOR-EV-A-DR-20000 – Rev P6); Block I, Elevation 02 (Ref: A766-FOR-EV-A-DR-20001 – Rev P6); Block I, Elevation 03 & 04 (Ref: A766-FOR-EV-A-DR-20002 – Rev P6); Block J, K, Elevation 01 (Ref: A745-FOR-EV-A-DR-20000 – Rev P4); Block J, K, Elevation 02 (Ref: A745-FOR-EV-A-DR-20001 – Rev P4); Block J, K, Elevation 03 (Ref: A745-FOR-EV-A-DR-20002 – Rev P4); Block J, K, Elevation 04 (Ref: A745-FOR-EV-A-DR-20003 – Rev P4); Block J, K, Elevation 05 & 06 (Ref: A745-FOR-EV-A-DR-20004 – Rev P4); Block L, Elevation 01 (Ref: A767-FOR-EV-A-DR-20000 – Rev P6); Block L, Elevation 02 (Ref: A767-FOR-EV-A-DR-20001 – Rev P6); Block L, Elevation 03 & 04 (Ref: A767-FOR-EV-A-DR-20002 – Rev P6); Block M, Elevation 01 (Ref: A746-FOR-EV-A-DR-20000 – Rev P5); Block M, Elevation 02 (Ref: A746-FOR-EV-A-DR-20001 – Rev P5); Block M, Elevation 03 & 04 (Ref: A746-

FOR-EV-A-DR-20002 – Rev P5); Block N, Elevation 01 (Ref: A757-FOR-EV-A-DR-20000 – Rev P4); Block N, Elevation 02 (Ref: A757-FOR-EV-A-DR-20001 – Rev P4); Block N, Elevation 03 & 04 (Ref: A757-FOR-EV-A-DR-20002 – Rev P4); Block O, Elevation 01 (Ref: A758-FOR-EV-A-DR-20000 – Rev P5); Block O, Elevation 02 (Ref: A758-FOR-EV-A-DR-20001 – Rev P5); Block O, Elevation 03 & 04 (Ref: A758-FOR-EV-A-DR-20002 – Rev P5); Block P, Elevation 01 (Ref: A759-FOR-EV-A-DR-20000 – Rev P7); Block P, Elevation 02 (Ref: A759-FOR-EV-A-DR-20001 – Rev P8); Block P, Elevation 03 & 04 (Ref: A759-FOR-EV-A-DR-20002 – Rev P7); Block Q, Elevation 01 (Ref: A747-FOR-EV-A-DR-20000 – Rev P5); Block Q, Elevation 02 (Ref: A747-FOR-EV-A-DR-20001 – Rev P5); Block Q, Elevation 03 & 04 (Ref: A747-FOR-EV-A-DR-20002 – Rev P5); Block R, Elevation 01 (Ref: A760-FOR-EV-A-DR-20000 – Rev P7); Block R, Elevation 02 (Ref: A760-FOR-EV-A-DR-20001 – Rev P7); Block R, Elevation 03 & 04 (Ref: A760-FOR-EV-A-DR-20002 – Rev P7); Block S, Elevation 01 (Ref: A762-FOR-EV-A-DR-20000 – Rev P5); Block S, Elevation 02 (Ref: A762-FOR-EV-A-DR-20001 – Rev P6); Block S, Elevation 03 & 04 (Ref: A762-FOR-EV-A-DR-20002 – Rev P5); Block U, T, Elevation 01 (Ref: A748-FOR-EV-A-DR-20000 – Rev P5); Block U, T, Elevation 02 (Ref: A748-FOR-EV-A-DR-20001 – Rev P5); Block U, T, Elevation 03 (Ref: A748-FOR-EV-A-DR-20002 – Rev P5); Block U, T, Elevation 04 (Ref: A748-FOR-EV-A-DR-20003 – Rev P5); Block U, T, Elevation 05 (Ref: A748-FOR-EV-A-DR-20004 – Rev P6); Block U, T, Elevation 06 & 07 (Ref: A748-FOR-EV-A-DR-20005 – Rev P5); Buildings E, F, G, H, I - Section A-A & B-B (Ref: A725-FOR-SC-A-DR-03201 – Rev P4); Buildings E, F, G, H, I - Section C-C (Ref: A725-FOR-SC-A-DR-03202 – Rev P4); Buildings, A, J, K, L - Section A-A (Ref: A725-FOR-SC-A-DR-03203 – Rev P4); Buildings, A, J, K, L - Section B-B (Ref: A725-FOR-SC-A-DR-03204 – Rev P4); Buildings, M, N, O, P, Q, R, S, T, U - Section A-A & B-B (Ref: A725-FOR-SC-A-DR-03205 – Rev P4); Buildings, M, N, O, P, Q, R, S, T, U - Section C-C & D-D (Ref: A725-FOR-SC-A-DR-03206 – Rev P4); Site Wide Elevation, 01 & 02 (Ref: A725-FOR-SW-A-DR-40000 – Rev P2); Site Wide Elevation, 03, 04, 05 & 06 (Ref: A725-FOR-SW-A-DR-40001 – Rev P2); Site Wide Elevation, 07, 08, 09, & 10 (Ref: A725-FOR-SW-A-DR-40002 – Rev P1); Illustrative Masterplan Phase 2 (Ref: A725-ALD-SW-L-DR-90001 – Rev P3); Landscape Masterplan Phase 2 - Ground Floor Layout (Ref: A725-ALD-GL-L-DR-90011 – Rev P3); Landscape Masterplan Phase 2 - Illustrative Ground (Ref: A725-ALD-SW-L-DR-90002 – Rev P3); Landscape Masterplan Phase 2 - Illustrative Podium (Ref: A725-ALD-SW-L-DR-90003 – Rev P4); Landscape Masterplan Phase 2 - Illustrative Roof (Ref: A725-ALD-SW-L-DR-90004 – Rev P4); Sitewide Sheet Layout (Ref: A725-ALD-SW-L-DR-90015 – Rev P1); General Arrangement – Key (Ref: A725-ALD-GL-L-DR-91010 – Rev P3); General Arrangement - Ground Floor Sheet 1 of 5 (Ref: A725-ALD-GL-L-DR-91011 – Rev P3); General Arrangement - Ground Floor Sheet 2 of 5 (Ref: A725-ALD-GL-L-DR-91012 – Rev P3); General Arrangement - Ground Floor Sheet 3 of 5 (Ref: A725-ALD-GL-L-DR-91013 – Rev P3); General Arrangement - Ground Floor Sheet 4 of 5 (Ref: A725-ALD-GL-L-DR-91014 – Rev P5); General Arrangement - Ground Floor Sheet 5 of 5 (Ref: A725-ALD-GL-L-DR-91015 – Rev P5); General Arrangement - Podium Garden above Block F, G, H (Ref: A725-ALD-01-L-DR-91021 – Rev P2); General Arrangement - Podium Garden above Block J, K, L (Ref: A725-ALD-01-L-DR-91022 – Rev P2); Biodiversity Roofs Planting Strategy (Ref: A725-ALD-RF-L-DR-91041 – Rev P6); Overall Landscape Edging Strategy (Ref: A725-ALD-XX-L-DR-91101 – Rev P4); Overall Landscape Furniture Strategy (Ref: A725-ALD-XX-L-DR-91102 – Rev P4); Overall Landscape Play Strategy (Ref: A725-ALD-XX-L-DR-91361 – Rev P2); Overall Soft Landscape Strategy – Trees (Ref: A725-ALD-XX-L-DR-91401 – Rev P5); Soft Landscape Overall Strategy - Planting Habitats (Ref: A725-ALD-XX-L-DR-91406 – Rev P6); Long section Through Town Square (Ref: A725-ALD-SC-L-DR-93611 – Rev P3); Long section Through Retail Street (Ref: A725-ALD-SC-L-DR-93612 – Rev P2); Long section Through Community Courtyard (Ref: A725-ALD-SC-L-DR-93613 – Rev P1); Long section Through South Fringe and Car Park (Ref: A725-ALD-SC-L-DR-93614 – Rev P1); Long Section Podium Garden Above Block F, G, H (Ref: A725-ALD-SC-L-DR-93615 – Rev P2); Paving Interface Detail Types P11 & P12 (Ref: A725-ALD-GL-L-DR-94111 – Rev P2); Paving Interface Detail Types P12, P14 & P15 (Ref: A725-ALD-GL-L-DR-94113 – Rev P2); Infrastructure Paving Detail Plan Sheet 01 (Ref: A725-ALD-GL-L-DR-94121 – Rev P1); Town Square Paving - Typical Detail And Interface With Water Feature (Ref: A725-ALD-GL-L-DR-94131 – Rev P2); Retail Street Typical Layout (Ref: A725-ALD-GL-L-DR-94132 – Rev P2); Secondary Connecting Street Typical Layout (Ref: A725-ALD-GL-L-DR-94133 –

Rev P1);Town Square Rasied Planter Typical Detail Southeast Side (Ref: A725-ALD-GL-L-DR-94134 – Rev P2); Retail Street Planter Typical Details (Ref: A725-ALD-GL-L-DR-94135 – Rev P1); Parkside Pedestrianised Street Details (Ref: A725-ALD-GL-L-DR-94141 – Rev P1); Parkside Residential Footpath Details (Ref: A725-ALD-GL-L-DR-94142 – Rev P1); Typical Details -Green Roof Landscape Buildup (Ref: A725-ALD-RF-L-DR-94401 – Rev P1); Typical Details - Green & Brown Roof Habitat Enhancement (Ref: A725-ALD-RF-L-DR-94402 – Rev P1); Typical Long Section - Green Roof (Ref: A725-ALD-RF-L-DR-94403 – Rev P1); Typical Long Section - Green & Brown Roof (Ref: A725-ALD-RF-L-DR-94404 – Rev P1); Earthworks Typical Detail: Soil Profiles - On ground (Ref: A725-ALD-GL-L-DR-94405 – Rev P1); Earthworks Typical Detail: Soil Profiles - Over Podium (Ref: A725-ALD-GL-L-DR-94406 – Rev P1); Earthworks Typical Detail Tree Pit Profiles - above ground (Ref: A725-ALD-GL-L-DR-94411 – Rev P1); Earthworks Typical Detail Tree Pit Profiles - above structure within planter (Ref: A725-ALD-GL-L-DR-94412 – Rev P1); Earthworks Typical Detail Tree Pit Profiles - above structure within paving (Ref: A725-ALD-GL-L-DR-94413 – Rev P1)

Supporting Documents:

Berkeley St Joesph Cover Letter; Residential Accommodation Schedule (Ref: A725-BSO-A SCH-00001 – Rev P1); Commercial Accommodation Schedule (Ref: A725-BSO-A-SCH-00002 – Rev P1); DAS Chapter 1 to 4 (Ref: A725-FOR-A-REP-00015 – Rev P4); DAS Chapter 5 (Ref: A725-FOR-A-REP-00015 – Rev P4); DAS Chapter 6 and 7 (Ref: A725-FOR-A-REP-00015 – Rev P4); DAS Chapter 8 Part 1 (Ref: A725-FOR-A-REP-00015 – Rev P4); DAS Chapter 8 Part 2 (Ref: A725-FOR-A-REP-00015 – Rev P4); DAS Chapter 9 Part 1 (Ref: A725-FOR-A-REP-00015 – Rev P4); DAS Chapter 9 Part 2 (Ref: A725-FOR-A-REP-00015 – Rev P4); DAS Chapter 10 and 11 (Ref: A725-FOR-A-REP-00015 – Rev P4); TGQ - Phase 2 - Car Park Management Plan 1 of 2 A725-TPP-G-REP-00001 – Rev P3); TGQ - Phase 2 - Transport Assessment 1 of 5 A726-TPP-G-REP-00002 – Rev P3); TGQ - Phase 2 - Transport Assessment 2 of 5 A726-TPP-G-REP-00002 – Rev P3); TGQ - Phase 2 - Transport Assessment 3 of 5 A726-TPP-G-REP-00002 – Rev P3); TGQ - Phase 2 - Transport Assessment 4 of 5 A726-TPP-G-REP-00002 – Rev P3); TGQ - Phase 2 - Transport Assessment 5 of 5 A726-TPP-G-REP-00002 – Rev P3); TGQ - Phase 2 - Daylight and Sunlight Report (Part 1) A725-HOD-G-REP-00002 – Rev P2); TGQ - Phase 2 - Daylight and Sunlight Report (Part 2) A725-HOD-G-REP-00002 – Rev P2); TGQ - Phase 2 - Daylight and Sunlight Report (Part 3) A725-HOD-G-REP-00002 – Rev P2); TGQ - Phase 2 - Daylight and Sunlight Report (Part 4) A725-HOD-G-REP-00002 – Rev P2); TGQ - Phase 2 - Daylight and Sunlight Report (Part 5) A725-HOD-G-REP-00002 – Rev P2); TGQ - Phase 2 - Energy Statement and Whole Life Cycle Carbon Emissions Statement A725-HOD-G-REP-00003 – Rev P1); TGQ - Phase 2 - ES Overheating Assessment and WLCA Appendix G1 A725-HOD-G-REP-00004 – Rev P1); TGQ - Phase 2 - Sustainability Statement and Circular Economy Statement A725-HOD-G-REP-00001 – Rev P1); TGQ - Phase 2 - Fire Statement A725-FDS-H-REP-00001 – Rev P1); TGQ - Phase 2 - Fire Strategy Report A725-FDS-H-REP-00002 – Rev P1); TGQ - Phase 2 - Planning Statement A725-ICI-T-REP-00001 – Rev P1); TGQ - Phase 2 - Statement of Community Engagement A725-BSO-G-REP-00001 – Rev P1); TGQ - Phase 2 - Mechanical Engineering Technical Note A725-SWE-T-REP-00001 – Rev P1); TGQ - Phase 2 - Acoustic Report A725-HAN-A-REP-00001 – Rev P1); TGQ - Phase 2 - Wind Microclimate Technical Report A725-GIA-H-REP-00001 – Rev P1); TGQ - Phase 2 - Commercial Demand Assessment A725-AND-G-REP-00001 – Rev P1); TGQ - Phase 2 - Economic Statement A725-ICI-T-REP-00002 – Rev P1); TGQ - Phase 2 - Commercial Update Letter A725-LUN-G-REP-00001 – Rev P1)

Type of Application: Reserved Matters

Application Received: 05/10/2021

Revised: 23/11/2021

Report by: Rohan Graham

Recommendation: That the Planning Committee GRANT approval of the reserved matters, subject to a deed of variation and the Conditions of Consent and Informatives contained within Appendix A.

EXECUTIVE SUMMARY

This planning application seeks permission for the approval of reserved matters in respect of Phase 2 of development as required under condition 2 of planning permission reference 171562VAR dated 16th October 2017 dealing with the matters of layout, scale, appearance and landscaping.

Previous reserved matters stages have been granted planning permission for Phase 1 (residential), Phase 2 (mixed use) and Phase 3 (residential), as well as the Spine Road and the Linear Park.

Phase 2 seeks to deliver 1,158 new homes (market and affordable), with associated parking, soft and hard landscaped public spaces and private amenity spaces. Additionally, 5,533sqm of commercial floorspace would be delivered. This proposal specifically excludes Buildings B, C and D which would include 112 dwellings and approximately 3863sqm of commercial floorspace, and these three blocks retain the existing consent with work having commenced. They are not under consideration within this application.

The proposal forms the revision and resubmission of development within Phase 2, which was previously approved under 185158REM dated 22/02/2019. The key changes since the previous Phase 2 Reserved Matters application are:

- An increase to the total number of residential dwellings by 362 dwellings; and
- A decrease to the total quantum of commercial floorspace, specifically:
 - o The removal of the hotel from this Phase;
 - o An alteration in the quantum of retail space; and
 - o An alteration in the size of the cinema.
- The provision of 'Affordable Workspace' as detailed in the Heads of terms below.
- The provision of a Community Centre, to be secured by way of a deed of variation

The key elements of the scheme as they relate to urban design, residential amenity quality, impact on neighbouring residential occupiers, affordable housing provision, highways, transport, parking, environmental health, Section 106 and Community Infrastructure Levy matters have had their merits assessed in accordance with the Outline Consent (171562VAR), development plan policies and any other material planning considerations.

The currently proposed development would be laid out in a manner consistent with the approved Outline Masterplan and in a similar manner to the previous Reserved Matters approval for Phase 2 (185158REM). The building scale conforms to the parameters set out in the outline planning permission and drawings provided in the Outline Masterplan application, detailing the maximum and minimum building heights and massing. The buildings would be predominantly finished with a variety of brickwork, similar to Phases 1 and 3, that would ensure a high-quality of architecture that would enhance the local area. The proposed landscaping for the public realm and private amenity space is considered to be of a high standard and would help establish a sense of place. There have been no objections from third parties.

On balance the proposal would accord with the relevant National, Strategic and Local Planning policies and guidance and approval could reasonably be granted in this case. It is not considered that there are any other material considerations, which would warrant a refusal of the application. It is therefore recommended that Reserved Matters be Approved with Conditions.

SUPPORTING DOCUMENTS

This application was submitted on 30th November 2020 and supported by a full set of Architectural Drawings and other supporting documents including the following:

- Application Form (prepared by Berkeley Homes (Southall))
- Covering Letter (prepared by Berkeley Homes (Southall))
- Planning Statement (Prepared by Icen Projects)
- Statement of Community Engagement (prepared by Berkeley Homes (Southall))
- Transport Assessment (prepared by TPP)
- Economic Assessment Report (prepared by Icen Projects)
- Phase 2 Outline Car Parking Management Plan (prepared by TPP)
- Commercial Demand Assessment (prepared by AND)
- Energy and Sustainability Statement (prepared by Hodkinson)
- Circular Economy Statement (prepared by Hodkinson)
- Whole Life Carbon Assessment (prepared by Hodkinson)
- Fire Statement (prepared by FDS)
- Daylight Sunlight and Overshadowing Assessment (prepared by Hodkinson)
- M&E Commercial Uses Note (prepared by SWECO)
- Acoustics Report (prepared by Hann Tucker)
- Wind and Microclimate Report (prepared by Gordon Ingram Associates)

RECOMMENDATION

That the committee **GRANT** approval of the reserved matters application, subject to conditions of consent contained in Appendix A and subject to the satisfactory completion of a deed of variation to the original Section 106 agreement, to secure the items set out below.

Heads of Terms

The proposed obligations and financial contributions to be secured by the deed of variation to the S106 agreement are set out as follows.

- Carbon offsetting contribution of £1,614,810;
- Post Construction Energy Monitoring contribution;
 - o £15,189 for the automated energy monitoring web-platform and associated officer/consultant time, and
 - o £7,644 for the cost of the energy monitoring equipment and data processing (4 years)
- The developer covenants to submit an ‘Affordable Workspace Plan’ for approval in writing from the Local Planning Authority (AWP). The AWP shall detail (but not exclusively):
 - o The provision of a minimum of 10% of the total commercial floorspace within Phase 2 of the development (or 250sqm, whichever is greater) to be provided as ‘Affordable Workspace’.
 - o Details on where and when the Affordable Workspace will be provided within Phase 2.
 - o Details of the degree of market rent discount, demonstrating a discount of at least 60% for year 1, 50% for year 2, 40% for year 3, 30% for year 4 and 20% for year 5 onwards – for each tenant).
 - o Details on how market rent, and the above discounts are calculated.

- Details demonstrating the provision of Affordable Workspace for a minimum of 15 years.
 - Details on who would benefit from the Affordable Workspace.
 - Confirmation of the proposed fit out specification, demonstrating that the affordable workspace would be fitted out to the same specification as the rest of the commercial floorspace.
- The developer covenants to provide Community Centre within Phase 2 (at a minimum of 700 square metres GIA at ground floor or a size otherwise by agreed by the London Borough of Ealing) whereby rents payable shall be peppercorn and service costs and repairs to be paid by the Developer.

SITE DESCRIPTION

The overall development site is approximately 44.7ha in size. The main part of the Site is located within the London Borough of Ealing. The Grand Union Canal (Paddington Branch) however, marks the boundary with the London Borough of Hillingdon (LBH) which encompasses land to the west of the Main Site that will be required for the construction of access links to the Site (both vehicular and pedestrian). The Western access from the A312 (Pump Lane) has now been completed and all site construction traffic to the site is using the route. The Eastern access, connecting the site with South Road, remains under construction.

The overall Site comprises a generally triangular parcel of land and is generally level at approximately 32 metres above ordnance datum (AOD). It has largely been remediated over a period of approximately 18 months under close scrutiny and monitoring by the Council and the Environment Agency as regards public health and ground water. There remains several phases of ongoing testing and monitoring.

Phase 1 (formerly Phase A) is mostly completed and occupied. Phase 3 has recently commenced works and remains under construction. Phase 2 (formerly Phase B) has begun construction with a basement currently being excavated. No above ground building works have commenced. The Phase 2 site has an area of approximately 5.25 hectares.

The Phase 2 sites adjoins Phase 1 and the Central Gardens, being located to the south of these two phases of development. The Cadent Gas (Retained Land) adjoins the site to the west. The wider urban area around the Site comprises mainly residential, retail and employment uses.

The applicant has rebranded the site from the former Southall Waterside to the Southall Green Quarter. All references in this report to ‘the Green Quarter’ or ‘Southall Green Quarter’ can be taken to refer to the site formerly known as ‘The Southall Gasworks site’ and ‘Southall Waterside’.

RELEVANT PLANNING HISTORY

The subject site was granted Outline Consent for the Masterplan of the site in September 2010. This Outline Masterplan has been varied on three occasions (not including non-material amendments) since February 2016. There have been five previous phases approved via reserved matters applications (one of which was the earlier revision of Phase 2).

The planning history is detailed below.

Reference No.	Date	Details	Status
216898NMA	12 April	Application for a Non-Material Amendment in (S96a) in	Approved

	2022	relation to planning permission reference 185158REM dated 25/02/2019 for 'Application for the approval of Reserved Matters pursuant to outline planning permission reference 171562VAR dated 16/10/2017, condition 2 pertaining to the second phase of development (Phase B): details relating to Layout, Appearance, Scale and Landscaping.' Application seeks to allow changes to the design of external areas and amendments to the use and layout of the ground floor space within Building B within Phase 2.	
212064NMA	26 March 2021	Application for the Non-Material Amendment in relation to planning permission reference 185158REM dated 22/02/2019 as amended by 201123NMA dated 08/04/2020 for 'Application for the approval of Reserved Matters pursuant to outline planning permission reference 171562VAR dated 16/10/2017, condition 2 pertaining to the second phase of development (Phase B): details relating to Layout, Appearance, Scale and Landscaping'. Application seeks to amend the architectural and facade treatment, landscape, internal layouts and unit mix as well as removing the podium to the rear of the building.	Approved
205101REM	23 April 2021	Application for the approval of Reserved Matters pursuant to outline planning permission reference 171562VAR dated 16/10/2017, condition 2 pertaining to the third phase of development (Phase 3): details relating to Layout, Appearance, Scale and Landscaping.	Approved
204493NMA	10 December 2020	Application for a Non-Material Amendment in (S96a) seeking to amend the approved Ground and Upper Floor Use Parameter Plans in relation to planning permission reference 171562VAR dated 16/10/2017 for 'Section 73 application for minor amendment to vary condition 3 (approved drawings) to amend the outline scheme drawings and removal of condition 41 (details of soakaways) of outline planning permission reference 170819VAR dated 07.09.17 for demolition of 22 houses; the remediation of the land and the redevelopment of the site to deliver a large mixed use development including residential, non-food retail, food retail, restaurants, bars and cafes, hotel, conference and banqueting, cinema, health care facilities, education facilities, office/studio units, car park, sports pavilion, an energy centre and associated car and cycle parking, landscaping, public realm, open space and children's play space. Full application: new access roads from the Hayes-By-Pass and Southall town centre to the application site for vehicle, cycle and pedestrian access, including drainage and a flood relief pond. Widening of South Road across the railway line, for the creation of a bus lane and three new accesses onto Beaconsfield Road. Two bridges over the Grand Union Canal and Yeading Brook to provide pedestrian and cycle access to Minet Country Park and Springfield Road. The quantum of development remains the same as the original scheme with the exception that the proposed development includes revised positioning of the hotel, supermarket and banqueting hall; the removal of the multi-storey car park to be replaced by basement car parking spaces; the removal of the east/west bus and taxi route through the Civic Square to be replaced by a dedicated pedestrian route and use for	Approved
195279REM	20 March 2020	Reserved Matters for Linear Park (now referred to as the Central Gardens).	Approved
194098NMA	3 October 2019	Non-material amendment to vary Condition 3 of 171562VAR relating to the approved vertical parameters for Phase 3.	Approved

193322NMA	2 September 2019	Non-material amendment to vary Condition 24 of 171562VAR relating to the delivery sequence for the South Road Bridge widening and improvement works to Merrick Road and the Eastern Access.	Approved
185126FUL	5 July 2019	Construction of an eight storey office building and basement of 7,454sqm (Use Class B1), associated cycle parking and associated works.	Approved
185158REM	22 February 2019	Application for the approval of Reserved Matters pursuant to outline planning permission reference 171562VAR dated 16/10/2017, Condition 2 pertaining to the second phase of development (Phase 2): details relating to Layout, Appearance, Scale and Landscaping.	Approved
185458NMA	10 December 2018	Non-material amendment to vary 171562VAR in relation to the Eastern Gateway.	Approved
178515NMA	23 January 2018	Non-material amendment to vary approved drawings to provide flexibility in the delivery of floor spaces.	Approved
172953REM	27 November 2017	Reserved Matters application for The Spine Road and the Straight.	Approved
171562VAR	16 October 2017	Section 73 application for Minor material amendment to vary condition 3 (approved drawings) and remove condition 41 (details of soakaways).	Approved subject to S.106 agreement
170819VAR	7 September 2017	Section 73 application for Minor material amendment to vary condition 3 to amend the Western Access detailed plans pertaining to the 2016 Masterplan Consent.	Approved subject to S.106 agreement
171094NMA	13 April 2017	Non-material amendment to vary the wording of condition 17 of 2016 Masterplan Consent to be consistent with minor material amendment consent PP/2016/0190.	Approved
160115REM	14 August 2016	Reserved matters application for Phase A.	Approved subject to S.106 agreement
PP/2016/0190	9 May 2016	Minor material amendment to vary condition 17 to enable commencement of preliminary works.	Approved subject to S.106 agreement
PP/2015/4682	18 February 2016	<p>Section 73 application for minor amendment to vary condition 3 to amend the outline scheme drawings pertaining to the 2010 masterplan in respect of outline approval for demolition of 22 houses; the remediation of the land and the redevelopment of the site to deliver a large mixed use development including residential, non-food retail, food retail, restaurants, bars and cafes, hotel, conference and banqueting, cinema, health care facilities, education facilities, office/studio units, car park, sports pavilion, an energy centre, multi-storey car park and associated car and cycle parking, landscaping, public realm, open space and children's play space.</p> <p>Full application: new access roads from the Hayes-By-Pass and Southall Town Centre to the application site for vehicle, cycle and pedestrian access, including drainage and a flood relief pond. Widening of South Road across the railway line, for the creation of a bus lane and three new accesses onto Beaconsfield road. Two bridges over the Grand Union Canal and Yeading Brook to provide pedestrian and cycle access to the Minet Country Park and Springfield Road application reference P/2008/3981-S dated 29 September 2010.</p> <p>The quantum of development remains the same as the original scheme with the exception that the proposed development now includes a police facility and public</p>	Approved subject to S.106 agreement

		toilets. In summary the proposed amendments to the scheme are: (1) re-distribution of public realm; (2) re-configuration of the internal street network; (3) re-positioning of the Civic Square; (4) re-locating the primary school; (5) improvements to the canal side; (6) re-configuration of buildings around the retained land; (7) alignment of western entrance on the gasworks site; (8) provision of a commercial access.	
PP/2015/4634	9 February 2016	Non-material amendment to Eastern Access and Beaconsfield Road Access	Approved
PP/2015/1376	21 July 2015	Dismantling of the MAN gas holder and subsequent installation of 2 No. temporary telecommunication masts (30 metres high) and a permanent single telecommunication mast (45 metres high).	Approved. MAN gasholder now demolished
PP/2014/4729	15 July 2015	Details of strategy and method of working for each phase of ground remediation pursuant to condition 36 of planning permission Ref P/2008/3981 dated 29/09/2010.	Approved
P/2011/4649	25 March 2014	Details of site wide construction management plan pursuant to condition 12 of planning permission Ref P/2008/3981 dated 29/09/2010.	Approved
PP/2013/4343	10 December 2013	Non-material amendment to rectify incorrect drawing references to LB Ealing conditions 3, 18, 19, 20, 25, 27 and 28.	Approved
PP/2013/4243	20 November 2013	Certificate of lawful proposed development for the demolition of all existing buildings and structures within the red line application boundary of planning permission ref: P/2008/3981-S as is necessary to implement the development approved by the same planning permission.	Approved
P/2008/3981	29 September 2010	Outline Application for demolition of 22 houses; the remediation of the land and redevelopment of the site to deliver a large mixed use development including residential, non-food retail, food retail, restaurants, bars and cafes, hotel, conference and banqueting, cinema, health care facilities, education facilities, office/studio units, sports pavilion, an energy centre, multi-storey car park and associated car and cycle parking, landscaping, public realm, open space and children's playspace. Full application: New access roads from Hayes by-pass and Southall Town Centre to the application site for vehicle, cycle and pedestrian access, including drainage and a flood relief pond. Widening of South Road across the railway line, for the creation of a bus lane and three new accesses onto Beaconsfield Road. Two bridges over the Grand Union Canal and Yeading Brook to provide pedestrian and cycle access to the Minet Country Park and Springfield Road.	Approved subject to S.106 agreement

THE PROPOSAL

The current application is for approval of Reserved Matters as set out under Condition 2 in respect of the second phase of development ("Phase 2") of the redevelopment of the Southall Gasworks site. Condition 2 states:

"No part of the development as hereby permitted shall commence until the reserved matters, as listed below, for that Part have been approved by the London Borough of Ealing as local planning authority:

- *Layout*
- *Appearance*
- *Scale*
- *Landscaping*

The relevant Part of the development shall in all respects be carried out in accordance with the approved reserved matters for that Part, unless agreed otherwise in writing with the London Borough of Ealing as the local planning authority."

The following Reserved Matters applications have been previously approved on site:

- Phase 1 (160115REM) approved in August 2016,
- Spine Road (172953REM) approved in November 2017;
- Phase 2 (185158REM) approved in February 2019;
- Linear Park (195279REM) approved in March 2020; and
- Phase 3 (205101REM) approved in April 2021.

This application forms the re-submission of the reserved matters for Phase 2. However, it specifically excludes Blocks B, C and D which were covered by the original approval (185158REM). These three blocks retain the existing consent with work having commenced. They account for 112 dwellings and approximately 3863sqm of commercial floorspace that remains outside of the calculations included within this report.

The key changes to this site since the previous Phase 2 Reserved Matters application are:

- An increase to the total number of residential dwellings by 362 dwellings; and
- A decrease to the total quantum of commercial floorspace, specifically:
 - o The removal of the hotel from this Phase;
 - o An alteration in the quantum of retail space; and
 - o An alteration in the size of the cinema.
- The provision of 'Affordable Workspace' as detailed in the Heads of terms below.
- The provision of a Community Centre, to be secured by way of a deed of variation

The consented masterplan proposes a legible structure of open space and public realm defining development plots, seeking to create a series of memorable spatial experiences around which distinctive new neighbourhoods can be delivered. This subject application phase seeks to create a new high-quality mixed-use development. The new residential and mixed-use blocks would be created around new landscaped spaces with pedestrian and cycle routes, which would link the uses to the wider area.

This Phase 2 application would provide a total of seventeen (17) blocks ranging in building heights from 7 to 19 storeys. The blocks within this application are identified as Buildings A & E - U (inclusively). Apart from Building A, these buildings have been renamed since the original Reserved Matters application.

Building A is identified as being a 'Marker Building', which would be the tallest within the Green Quarter site at 19 stories. It would have frontages to both the Central Gardens and the Town Square and would be visible from the wider area.

The new homes would range in size from studio/suites up to 4-bedroom apartments which would be delivered in a range of tenures in accordance with the approved masterplan and the associated Section 106 agreement.

The buildings would incorporate a materiality that is characteristic of this area with a range of coloured brickwork. Balconies would be provided for each flat providing private amenity space for every dwelling.

The new buildings would be laid out around a street pattern that is reflective of that developed within Phase 1 and would run parallel and perpendicular to the street network to the north. It would provide interconnection between the Spine Road and The Straight.

Parking would be available within the basement, along the new streets and within the landscaped land to the south. The public domain would incorporate a large town square centrally within the site as well as

Having regard to the above, it is reinforced that the purpose of this application is to assess the layout, appearance, scale and landscaping of the proposed built form. The principal of the development and building parameters have been previously established. The planning obligations, such as affordable housing, have been identified within this application, but would be subject to further consideration prior to the occupation of the development and within an ongoing manner across the course of The Green Quarter site.

The proposed residential units and tenure typologies that Phase 2 seeks to deliver are set out below in Table 1 below.

Table 1: Residential Uses (Tenure type and size)

Unit Type	Suite	1B2P	2B3P	2B4P	3B5P	3B6P	4B6P	Total
Private	28	463	167	198	73	14	0	943
Shared Ownership	0	23	23	32	12	0	0	90
Affordable Rent	0	25	12	54	24	0	10	125
Total	28	511	202	284	109	14	10	1158

Phase 2 is identified as the primary commercial centre within the Southall Green Quarter site. This revised application seeks to establish 5,534sqm of commercial floorspace across retail, office and cinema. An additional 3863sqm would also be provided within the adjoining Buildings B, C and D to complete to Town Centre.

The proposed commercial tenancy summary is outlined within Table 2 below.

Table 2: Commercial Accommodation Summary

Unit Type	Number	GIA Area (sqm)	GIA Area (sq ft)
Retail E(a)	14	2532	27254
Office E (G) (i)	2	924	9942
Sui Generis (Cinema)	1	736	7922
Health Centre E (e)	1	1028	11,065
Sui Generis (Pub)	1	314	3,380
Total Commercial Units	19	5534sqm	59,564sq ft

The basement car park would primarily provide the residential car parking with visitor car parking being provided on street. Within the overall development, there would be a total of 428 long stay car parking spaces for residences. 400 of these spaces would be located within the single basement below Buildings A – K. The provision includes 49 wheelchair accessible bays.

The total car and motorbike parking provision within Phase 2 is as follows.

Table 3: Car & Motorcycle Parking Provision

Location	East Basement	Southern Fringe West	Southern Fringe East	Arrival Square Area	Western Boundary	Total
Private Standard Residential Spaces	375	6	0	0	0	381
Private Residential WC Spaces	33	4	0	0	0	37
EM Spaces (WC Spillover)	3	0	0	0	0	3
AH Residential WC Spaces	0	18	0	0	0	18
Standard Commercial Spaces	0	0	23	7	0	30
Commercial WC Spaces	0	0	8	4	0	12
Car Club Spaces	0	0	2	0	2	4
Health Centre Spaces	0	0	10	0	0	10
Residential Visitor	0	0	8	0	20	28
WC Residential Visitor	0	0	0	0	2	2
Total	411	28	51	11	24	525

The proposal also seeks to ensure that a cycle friendly environment would be provided with cycle parking spaces being distributed across the site in secure cycle stores for residents and within the public realm for users of the commercial premises. A total of 2,185 secure and covered cycle parking space, as well as 47 staff long stay spaces and at least 110 visitor spaces will be provided.

Phase 2 incorporates areas of hard and soft landscaping with paving and tree planting surrounding the new roadways. The buildings roofs would be covered with a green roof and Photovoltaic solar panels.

Amendments

Amended plans were lodged on 23/11/2021. These revisions were lodged in response to the comments raised by the Design Review Panel and Council Planning Officers. Specifically, the following revisions were made to the plans:

- The reduction in the width of building A.
- The reduction in the height of buildings T and O by 1 storey and a reduction in height of buildings S, P, L and I by 2 storey’s.
- The space between buildings M, P, S and R; and the space between buildings S and T were increase by between 0.4 and 3.6m.
- Change in external materiality of several buildings.

In addition, the developer has since committed to provide affordable workspace within Phase 2, including the provision of a Community Centre (of approximately 700sqm) at nominal rental.

These amendments have overall resulted in an improvement to the design and functionality of Phase 2 and have sought to increase the level of public benefit through the incorporation of affordable workspace and a Community Centre. These amendments do not however fundamentally alter the nature of character of the development and represent a reduction in the quantum of development. As such, it was not deemed necessary to re-consult the application.

STATUTORY CONSULTATION

Neighbour Notification:

The application has been advertised as a major planning application; affecting an area of wider interest. Site notices displayed on 27/10/2021 (expired on 17/11/2021). A Press notice was published on 27th October 2021 (expired on 17th November 2021). Letters were sent on 22nd October 2021 to Southall Broadway and Southall Green ward councillors.

No written representations have been received.

External

Letters sent on 22/10/2021 (expiring on 12/11/2021) to:

- Southall History Society;
- Save Our Southall;
- Health & Safety Executive;
- Civil Aviation Authority;
- Thames Water;
- Canal Rivers Trust;
- London Borough of Hillingdon;
- London Borough of Hounslow;
- Havelock Estate Residents Association;
- Toplocks Residents Association;
- Transport for London;
- Historic England Archaeology Advisor (GLAAS);
- Historic England;
- Environment Agency;
- National Grid Plant Protection;
- London Fire & Emergency Planning; and
- Metropolitan Police Service.

Four (4) written representations were received. These are outlined below.

Table 4: External Referral Responses

Written representation	Case Officers response
<u>Canal and River Trust</u> No comment.	Noted.
<u>Historic England</u> No need to consult GLAAS.	Noted.
<u>Transport for London (TfL):</u> On a general point although table 2 in the Transport Assessment provides a review of policy compliance, we would appreciate a clearer indication of where these points are addressed in the submission including signposting to sections of relevant documents and plans. This is particularly important given the lack of pre-application engagement with TfL and the need to liaise with colleagues.	The applicants reviewed these comments from TfL and have responded to them in turn. Several amendments were

<p><u>Healthy Streets</u></p> <p>We would expect the application to demonstrate more clearly how the Healthy Streets Approach as set out in policy T2 of the London Plan is being applied in the design and management of streets and routes for pedestrians and cyclists. This is particularly important given the changes that are envisaged as part of the reserved matters application, It would be helpful to understand how the proposed buildings and layout of phase 2 relate to walking and cycling infrastructure approved under previous applications. We need to be able to assess the detailed design of routes through the phase 2 area including widths, segregation between different users and access routes to buildings for cyclists. We would welcome some guidance on where this information can be found.</p> <p><u>Bus Infrastructure</u></p> <p>Given that this will supersede the previous reserved matters application 185158REM we seek assurances that bus infrastructure and facilities agreed as part of previous applications will still be provided. This includes the bus standing area and drivers' facilities in building B which does not form part of the current application. We also seek assurance that agreement has been reached with colleagues in TfL London Buses regarding the location and detailed design of bus stops shown in the Design and Access Statement but not included in the Transport Assessment. We need to confirm that all bus infrastructure is in full accordance with TfL guidance on bus stops. The design needs to take into account provision for and any potential conflicts with general traffic, servicing as well as cyclists and pedestrians.</p> <p>We also seek clarity on proposed bus routeing through phase 2. The section of spine road appears within the red line boundary on some drawings but appears to be excluded on others. There is reference to servicing bays on the spine road although these appear to lie beyond the red line boundary. We seek clarification on whether this road will be available for buses to use and whether any bus stops or other facilities will be provided. A map showing details of all provision for buses would help in this regard.</p> <p><u>Taxi provision</u></p> <p>We also seek assurances that the location and design of the proposed taxi rank has been agreed with the Public Carriage Office because we understand from section 7.1.3 of the Transport Assessment that the taxi rank has been moved from the previously agreed location. It is unclear from the description and drawings whether this is intended to be a full taxi rank where taxis can wait for customers (as suggested on some drawings) or whether it is simply a drop off point for taxi passengers.</p> <p>Section 5.1 of the Transport Assessment states that 'There is a taxi rank capable of accommodating two taxis located to the south of building E.' and shows this in inset 5.1. However, it is not clear from the drawings how the space for taxis will be differentiated from the adjacent loading bay or how two taxis will be able to access it simultaneously when the loading bay is occupied (or vice versa). It is important that the space is available for two taxis to use at all times and that it is not blocked by delivery vehicles or used for servicing. We would appreciate details of liaison and confirmation that the design, dimensions and location of the taxi rank has been approved by the Public Carriage Office.</p> <p><u>Road safety audits</u></p> <p>We seek clarity on the revised road safety audits that have been carried out as</p>	<p>agreed to in response to TFLs comments.</p>
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part of the phase 2 submission. This is particularly important given changes to traffic circulation and the design or location of facilities for cycling, buses, taxis and servicing vehicles since the previous phase 2 planning consent.

Transport Assessment and Travel Plan

We question whether the trips by mode for the residential development presented in sections 6.3.3 and 6.3.4 of the Transport Assessment are accurate because the car driver share exceeds the planned provision of car parking. We believe that there will be a higher generation of trips by bus and rail (including a combination of the two) than previously envisaged due to the lower provision of car parking. The revised analysis would be helpful in planning sufficient bus capacity to serve phase 2.

There is reference in section 5.11 of the transport assessment to an outline travel plan being submitted but this does not appear in the uploaded documents, nor does it appear in the list of submissions in the planning statement. We would like to have the opportunity to review this document.

Car and motorcycle parking

Although we welcome the reduction in car parking from the level permitted in the outline permission we would like to see the amount of general parking reduced further given that most of phase 2 is in close proximity to Southall station and will have some of the highest PTALs so there is greater potential for car free living. We also believe that the number of visitors' spaces for both the residential and commercial developments should be substantially reduced. There is no allowance in London Plan policy T6.1 for residential visitors' parking and the justification for commercial visitors' parking is weak, given the smaller-scale of commercial facilities now planned which are likely to serve a more local catchment where active travel will be a realistic option.

An outline car parking management plan has been submitted although it does not provide details of the allocation process for residents' car parking spaces. London Plan policy T6.1B requires that 'Parking spaces within communal car parking facilities (including basements) should be leased rather than sold. The car parking management plan should confirm that this requirement will be met.

We do not see the need or justification for provision of 58 motorcycle parking spaces. London Plan policy T6F states that 'Where provided, each motorcycle parking space should count towards the maximum for car parking spaces at all use classes.' so this would result in a total of 605 parking spaces rather than 547. We strongly recommend that some of the area proposed for motorcycle parking would be better utilised to increase provision of the more accessible Sheffield stands for cycle parking.

Cycle parking

We welcome provision of cycle parking that meets the minimum requirements of policy T5 in the London Plan. However, there is a lack of information on how qualitative standards including those set out in London Cycling Design Standards (LCDS) have been met. Section 5.7.2 of the Transport Assessment states that '5% of cycle parking will be suitable for use by larger/non-standard bikes and provided in the form of Sheffield stands.' We would recommend a further 15% of spaces are provided as conventional Sheffield stands in addition to the 5% for adapted cycles because not everybody is able to access two tier stands.

There is also a lack of information on how access will be gained to the proposed cycle parking stores. Cycle lifts are generally preferred to ramps. A minimum of two cycle lifts should be provided to cater for lift failures If ramps

are the only option they should be of a shallow gradient and cycle access must be segregated from vehicle access. Access to the cycle stores should be internally from the building core rather than through external entrances which may pose a security and personal safety risk. A clearer set of maps showing the location and design cycle parking for each building should be included as part of the submission.	
TfL – Crossrail (Safeguarding): No Comment	Noted
<p>Met Police Research on Police.uk has highlighted that crime is occurring directly in the immediate vicinity of the proposed development, including serious crimes such as violent crime, burglary and motor vehicle crime, and a high proportion of Anti-Social behaviour.</p> <p>As such, serious consideration must be given to crime and anti-social behaviour at the proposed development site.</p> <p>Throughout the development Berkeley Homes have not engaged with Secured by Design or the designing out crime team. The first phase of Southall Gas works has been completed and failed to achieve Secured by Design certification. A private security company has now been employed to respond to the anti-social behaviour for the occupied blocks, due to the lack of security features from the SBD Homes guides such as third-party security tested doors and access control. I am concerned the remainder of the development will also fail to achieve Secured by Design certification due to the lack of engagement from the developer.</p>	A condition within the Outline Consent requires Secured by Design to be considered and discharged prior to the commencement of works. Therefore, its consideration sits outside of the assessment of this application.

Internal Consultees:

Two (2) internal referral responses were received. These are outlined below.

Table 5: Internal Referral Responses

Consultee	Comments
Energy	<p>The Council is broadly supportive of the proposed energy strategy. The Strategy has been assessed against the draft SAP10 benchmark and follows the standard energy hierarchy of “Lean, Clean, Green”, and is in line with London Plan policy S12 & S13, and Ealing DPD policy 5.2.</p> <p>The Strategy states that the shell & core office space will achieve BREEAM “Excellent” (76.27% score) standard, and the Retail space “Very Good” (59.70%).</p> <p>A Dynamic Overheating assessment with proposed mitigation measures has been carried out (Hodkinson September 2021 v3). The analysis was assessed against CIBSE TM59 and the DSY1 (average summer) weather data file, as well as the more intense (but non-mandatory) DSY2 (2003) and DSY3 (1976) data files.</p> <p>There is no available “Clean” district heat network (DHN), but the energy plant room will be futureproofed for connection to any future DHN through the installation of a valve and heat exchanger.</p> <p>Currently, the overall site-wide CO2 emissions will be cut by at least 52.94%, with 14.66% carbon reduction through “Lean” efficiency measures, 36.08% “Clean” reduction through connection to the sitewide heat network, and 2.2% through “Green” renewable energy PV panels.</p> <p>There is a shortfall of 16,998 tonnes CO2 (over 30 years) in the zero-carbon that will be mitigated through an “offset” S106 payment at £95 per tonne to the Council of £1,614,810. For information, the carbon offset amount saved through the Clean/Green energy equipment is £1,313,565.</p> <p>If after three years of in-situ monitoring the renewable/low-carbon energy systems do not deliver, within a reasonable margin of error, the carbon reductions predicted in the Energy Strategy then the Developer will need to pay an additional Carbon Offset contribution to mitigate some or all of the shortfall.</p> <p>Ealing Council also requires the additional physical monitoring and performance analysis of the renewable/low-carbon energy equipment. A S106 payment will be sought for the</p>

	<p>implementation of the energy monitoring policy. In particular, policy SI2 that requires new major development to meet zero-carbon standards with at least a 35% CO2 reduction beyond Building Regulations Part L 2013 (or any later version) being achieved onsite. Any shortfall will be met through a S106 carbon offset contribution. The following conditions are recommended:</p> <ul style="list-style-type: none"> - Condition XX: Energy and CO2 - Condition XX: Post-construction renewable/low-carbon energy equipment monitoring - Condition XX: Post-construction energy use monitoring (“be Seen”) - Condition XX: Non-Residential BREEAM energy/CO2 accreditation - Condition XX: Whole Life-Cycle Carbon Assessment
<p>Environment Protection Officer</p>	<p>Councils Environment Protection Officer provided the following</p> <ul style="list-style-type: none"> • This BS 4142 assessment concludes that noise from the PRS will result in a significant adverse impact at the nearest Phase 2 dwellings, with the Rating Level exceeding the Background Sound Level by +29 dB. This is in the same ball park as identified during recent noise survey and assessment to the Phase 1 dwellings, where the noise from the Cadent PRS exceeded the background by +25 dB. • Section 9.5 of the report concludes that the PRS is an issue and has the potential to result in problems and complaints going forward. Whilst Hann Tucker hint at the possibility of reviewing the layout of the nearest blocks to reduce the number of noise sensitive rooms facing the PRS, it appears that the key mitigation measure that they are relying upon is mitigating the PRS at source. In section 9.5 they state “<i>We understand Berkley Homes (Southall) Ltd are in dialog with the Cadent with the view of mitigating at source.</i>” Whilst it could be possible to design out this issue at the receptor (by having significantly enhanced glazing, and a ventilation and overheating strategy that essentially does not require windows to be opened at all). Therefore an at source solution is most likely. • Condition 79 of the outline masterplan permission 171562VAR is the most relevant with respect to noise. The reference to Category B & C noise exposure is a reference to old planning policy (PPG24) which predates the 2012 NPPF and 2010 NPSE and is only relevant to transportation noise source. However later on, the condition goes on to state that prior to the commencement of each phase, details shall be submitted for mitigation works to protect sensitive rooms, gardens and balconies from “external noise”. This is pretty vague, and could it be argued that this is a wide enough description to include the PRS as a source of external noise? • Therefore, what I’d like to understand is: <ul style="list-style-type: none"> ○ What is the current status of the dialogue between Berkeley and Cadent to resolve this at source? This noise report was written in 2018, and therefore I would hope a progress update could be provided. ○ Hann Tucker acknowledge in this noise assessment that there will be a significant adverse impact unless the Cadent PRS noise is mitigated. Therefore could we look to negotiate with Berkeley to add a condition to this reserved matters application which secure appropriate internal noise levels inside the Phase 2 affordable units located closest to the Cadent PRS. In reality it is most likely that an at source solution will be implemented, but since we cannot condition this due to it being outside the RLB, this alternative should provide the Council with the assurance that the matter will be addressed prior to the occupation of any Phase 2 units.

Unlike the previous Reserved Matters application on the site, the proposal was referred to the Design Review Panel on two occasions as well as the Community Review Panel. The comments received from these panels is summarised below.

Table 6: External Referral Responses

Panel (date)	Case Officers response
<p>Design Review Panel (dated 13th July 2021)</p>	
<p><u>Summary</u> The proposals contain many positive elements, however the panel</p>	<p>Noted. These comments have been considered by the</p>

<p>has concerns about the proposed increase in density, particularly with regard to the relationships between buildings and the impact on public and amenity space.</p> <p>It feels that a rigorous shading analysis will be essential to ensure that the massing and layout of the scheme do not compromise the quality of the public realm and of the residential accommodation.</p> <p>The approach to the location of the commercial units appears sensible and the ambition for the landscape is welcome. However, there needs to be a clearer hierarchy to the public realm, both in terms of character and of use, and the panel notes that the effective management of these spaces will be hugely important to the success of the scheme. Further, it feels that the scheme would benefit from a more heterogeneous architecture that supports the distinctiveness of the different areas proposed.</p>	<p>applicant and addressed prior to the second DRP meeting.</p>
<p><u>Overall approach</u></p> <ul style="list-style-type: none"> • The panel recognises that the site is the subject of an extensive planning history, to which it was not party. Consequently, its comments here are restricted to the scheme as presented at this review within the scope of the existing reserved matters. • Noting that, the panel would like to have seen the revised sustainability strategy for the scheme, to ensure that it is still appropriate to the amended proposals. 	<p>Noted. These comments have been considered by the applicant and addressed prior to the second DRP meeting.</p>
<p><u>Layout and scale</u></p> <ul style="list-style-type: none"> • This is a very dense scheme and the panel notes that the proposals under consideration here are primarily to replace a substantial amount of commercial space with additional residential units. However, the increase in residential density does not appear to be reflected in the layout, especially in relation to the provision of public and amenity space, which the panel feels could be under significant pressure. • The layout consequently feels crowded, particularly towards the western end of the site, and combined with the significant heights and massing proposed, this creates very close relationships between the buildings. This is likely to lead to overlooking and overshadowing, thereby compromising the quality of the residential accommodation provided. • Rigorous shade analysis is essential and this should, in particular, test the impact on the intended functions of spaces, with the layout optimised for amenity. • The heights and massing of the buildings feels quite homogenous, and the scope for introducing greater variation should be considered. • The panel feels that relaxing the scale and layout of the scheme to create space would allow greater elegance to the buildings and ensure adequate daylighting to the units and to the public realm. This could also inform greater variation in the scale of the spaces between buildings. • The panel questioned the need for the concentration of parking at the southern edge of the site and the amount of roadway associated with it. This feels like a departure from the project's ambitions with regard to sustainability and the creation of a rich landscape. 	<p>Noted. These comments have been considered by the applicant and addressed prior to the second DRP meeting.</p>
<p><u>Landscape</u></p> <ul style="list-style-type: none"> • There are many positives in the landscape design, but more work to differentiate the character of the two parts of the site – Quarter Yard and Parkside - would be beneficial. Much of the public realm is formed of linear spaces that are not 	<p>Noted. These comments have been considered by the applicant and addressed prior to the second DRP meeting.</p>

<p>clearly defined, in use or in form.</p> <ul style="list-style-type: none"> • In general, the panel feels that greater clarity is required on the hierarchy of the communal and public spaces, as well as a better understanding of how they will be used by different groups. Further work is also required to define and develop the thresholds to these spaces. • The landscape along the southern fringe of the site appears underdeveloped and the panel would like greater consideration to be given to its character. • Furthermore, the location of so much car parking here undermines its potential to develop much value in terms of place making or amenity, which the panel feels is a missed opportunity. • The town square and its mirror pool could be a very positive feature of the proposals: the scheme is a critical part of a new city quarter and the town square has the potential to offer an important dwell space for residents and for visitors, including those using the station. • While the proposed design has the potential to create an attractive space, the panel questions the rationale for introducing soft landscape between the buildings and the town square, effectively severing the relationship between the two. Without this activation, the panel is concerned that the space could become neglected and underused. In any case, active management of the town square will be critical. • In particular the panel did not understand the role of the landscaped 'apron' at the base of Building A. • The panel feels activity should be more focussed in the truly public areas, such as the town square, rather than being spread across the wider public realm. This will both help to define the hierarchy of spaces, and also ensure genuine vibrancy at key points. • The panel supports the scheme's ambitions for greening, but would like a stronger understanding of the street frontages, particularly in regards to their activation and treatment. • The panel would also like to understand better the contribution of the landscape to the scheme's biodiversity ambitions, along with the detail of how the SUDs and green roofs will function. • The panel notes that no information was provided on the play strategy for the scheme, and would like reassurance that this is in place and adequate. 	
<p><u>Appearance</u></p> <ul style="list-style-type: none"> • The panel feels that the proposed scheme is quite homogenous in appearance and it questions the extent to which the ambition to create distinctive character areas in Quarter Yard and Parkside has been realised. • The panel is unconvinced by the attempts to reference industrial heritage in the Quarter Yard area: traditional industrial buildings are generally horizontal not vertical. Rooting its character more clearly in the industrial heritage of the site would help to make more apparent the transition to the natural character of Parkside. • The panel also feels that an opportunity has been missed to build on the character of the water tower, which does not appear to be clearly reflected within the proposals. • Building A requires further refinement, if its prominence is to be justified. It should be unapologetically treated as an object building, with a more slender profile that sets it apart as a genuine marker element and with a more robust materiality. 	<p>Noted. These comments have been considered by the applicant and addressed prior to the second DRP meeting.</p>

<ul style="list-style-type: none"> Similarly, the panel would like to see greater attention paid to materiality across the scheme, introducing variation, especially in order to differentiate Parkside and Quarter Yard. 	
Design Review Panel (dated 7th December 2021)	
<p><u>Summary</u> The panel commends the thorough, rigorous presentation and feels that the comments raised at the previous review have been addressed clearly. It feels that the scheme has improved and that the reduction of building heights is positive. However, it feels that the articulation of the different blocks, interaction between the residential uses and public realm at ground floor, as well as the development of a clear identity for this new neighbourhood could be further improved.</p> <p>The narrative for the different character areas is good, but the panel feels that the blocks still feel very homogenous and should be more clearly defined. The panel suggests that giving a clearer identity and character to the different buildings could help. It notes that Blocks E and J have started to explore this reasonably well, and the panel urges the design team to continue developing this approach and apply the same level of detail to the whole of the masterplan.</p> <p>The panel feels that the number of stacked flats and bolt-on balconies is a particular issue and should be reconsidered. Further testing of different typologies should also be considered, to add interest and activity to the elevations.</p> <p>The panel feels that the public realm and landscape design have improved significantly. The variety of character areas, street trees and different scales is well thought through, but the panel still has concerns relating to the interaction with the residential uses at ground floor. It feels that the building edges and blank facades would benefit from further consideration and suggests that placing individual front doors and gardens onto the residential streets could help enliven the space.</p>	<p>Noted.</p>
<p><u>Layout, height and massing</u></p> <ul style="list-style-type: none"> The panel notes that Block A has improved, and that the reduction in width has made it more elegant. It suggests further articulation of this block, by setting back to the core at the upper floors, could help break down the mass when viewed from a distance. The panel feels that the cranked building, comprising Blocks T and U, is disappointing when compared to the other buildings in the Parkside character area. Given the affordable units are located here, it notes that these deserve more care and attention to ensure that the development is tenure blind. Given the east-west walking route and view corridor between the Parkside group of buildings, the panel feels that the cranked building provides an opportunity to end the vista on something more special and distinctive. It would encourage the design team to consider splitting Blocks T and U near the crank axis, to provide visual and physical connections as well as an opportunity for improved daylight into the courtyard space. The panel questions the relationship between Blocks R and M and the park frontage. It feels that an orthogonal orientation could better define this edge and provide more residents with better park views. The panel also has concerns regarding the separation 	<p>Noted. These matters will be considered further below.</p>

<p>distances between the blocks, and it feels that the minimum 8 metres proposed is rather tight.</p>	
<p><u>Materials and articulation</u></p> <ul style="list-style-type: none"> • The panel notes that the articulation and architectural language has improved since the last review, but it feels that the design team could be more ambitious and add more playfulness and character to the masterplan. • The panel feels that further work is needed to articulate the facades, to add more depth and modelling, to introduce more texture to the elevations rather than simply patterning. • The articulation of Block A is still quite bland, and the panel would encourage the design team to make this landmark building more expressive and special. • The narrative of the two character areas is clear but the panel feels that, when viewed together from the south, the difference between them isn't expressed clearly and that when viewed from a distance, the difference in brick tones may be too subtle. • The panel recommends considering further precedents, for example the London Legacy Development Corporation Athletes Village (now East Village), as well as the Hackney Wick masterplan. The panel notes that these developments had regimented masterplans and strict design codes, but worked successfully to differentiate the buildings within the wider neighbourhood, which could be an appropriate approach here. • The panel feels that the persistent stacking of flats creates a level of homogeneity through repeated windows, similar proportions, and bolt-on balconies. The balconies are particularly problematic, and the panel suggests considering alternative solutions, for instance using recessed or partially recessed balconies for the more public routes and civic frontages. • The base is well defined, especially for the retail units, but the panel feels that for the residential buildings this approach is less meaningful. It suggests that introducing different typologies, with maisonettes or similar at the lower two floors would be a better use of this feature and give it more purpose. • The panel feels that the palette of materials picks up on industrial heritage of the place but notes that Southall is a very diverse part of London. It would like to see how the cultural heritage could be brought into the articulation of the buildings to create more of a sense of place. 	<p>The materials would generally be in line with those envisaged under the Outline Consent and those previously approved for Phase 2. This is discussed in greater detail below.</p>
<p><u>Ground floor uses</u></p> <ul style="list-style-type: none"> • The panel has concerns about the interface between architecture and public realm on the 'link street' in the Parkside character area. It feels the design team need to think more about the façade treatment here, and the panel notes that the extent of bins and bike storage should be reconsidered to ensure that the buildings provide activity on this street. • The panel feels that introducing more private front doors and gardens would add a level of interaction, as well as provide passive surveillance and overlooking, which would help the space feel more like a residential neighbourhood. • The panel has concerns that there is a lot of retail space provided. It would welcome further detail to understand the market for these units, to ensure that there is a strategy for how these will be occupied to ensure building frontages are 	<p>The appearance of the buildings will be considered further below.</p>

<p>active and that the places are well used.</p> <ul style="list-style-type: none"> • It suggests that the retail character could also be improved by adding recessed areas that provide shelter and more clearly defined entrances. • The panel feels that there is a lack of a central hub for the community, and it would like to see consideration of whether some of the retail space could be given over to community uses. 	
<p><u>Public realm and landscaping</u></p> <ul style="list-style-type: none"> • The panel feels that the integration of the buildings with the landscape design has improved significantly, but it is still not convinced by the planted wrap-around at Block A. Opening this up, to provide more views out from building and links to square, would be better and it would like to see further consideration given to how this could work with the level changes. • The narrative for the public square is convincing and the scale precedents are helpful, demonstrating the variety of uses that could be accommodated in this space. However, the panel has concerns that this would require a significant commitment to active management and questions whether this has been considered fully. It is certainly reassured by Berkeley’s stated commitment to managing the public spaces. • It suggests that engaging with the community could help to ensure that it is successfully used and managed. It notes that further consultation could also help to better represent the diversity of the local community, which the panel feels is currently not clearly reflected in the proposals. • The distinctiveness between the landscape character areas, surface treatment, scale of spaces and relationship to building materials has improved and works well. • The panel feels that the tree strategy is successful and that the variety and differentiation of species could help act as a wayfinding device, as well as provide enhance biodiversity opportunities. • The panel questions the approach to play space, given that there are several areas that seem to be within the private gated courtyards. It would like to see further detail of the access arrangements for these spaces. • Adding incidental play to the pedestrian streets alongside the retail units should also be considered further. • The panel notes that there is significant amount of shared hard landscape but feels that there should be more ‘soft places’ to occupy. • The panel has concerns relating to the treatment of building edges. Suggesting that the landscaped edge at the building boundaries is a little undefined and doesn’t make a convincing residential street character. 	<p>It is noted that the Panel believe that the landscape design has improved significantly.</p>
<p><u>Parking</u></p> <ul style="list-style-type: none"> • The panel recognises that the parking strategy has been defined by the outline application, however it feels that the extent of surface parking is still disappointing, despite the reduction since the previous review. It questions whether there is scope for further reducing the amount of car parking in the Southern Fringe, suggesting that less could be provided up front and monitored to understand the actual demand and proven need over time. • It asks whether the car parking could be redistributed as street parking, to allow more opportunities for incorporating 	<p>Parking remains covered by the Outline Consent. The applicants have proposed parking that is well below the 0.7 spaces per unit allowed under the Outline Consent.</p>

<p>within the landscaping and providing more green spaces in place of the parking to the Southern Fringe.</p>	
<p>Community Review Panel (dated 14/12/2021)</p>	
<p><u>Summary</u> The panel feels that the general layout and heights of the buildings seem appropriate, but it has concerns regarding the scale and dominance of the landmark building, Block A. It feels that this tower is too tall and would impact negatively on the quality of life for those living there, as well as the public realm and streetscape.</p> <p>It notes that the landscape design is well considered and delivers a variety of spaces that could add character and identity to the space. The panel would like to see further detail of the more private areas, including the courtyard and podium gardens to understand how these spaces will be used by the community, with areas for leisure, play and growing. It would also like to see further consideration of opportunities for integrated informal play within the street design, as well as within the central square.</p> <p>The panel is pleased to see that the health centre will come forward in this phase, but it has concerns that, given the number of families and children living here, there will be additional pressure on other existing infrastructure in the local area. It suggests looking at opportunities for bringing forward an education use in this phase, such as the proposed school identified in the later development of the masterplan.</p> <p>The panel feels that the approach to sustainability is disappointing and urges the applicant team to be more ambitious to meet recent targets that address climate change, which exceed the required targets noted in the 2010 planning approval. These comments are expanded below.</p>	<p>Noted. These will be addressed within the detailed comments below and within the assessment.</p>
<p><u>Building heights and density</u></p> <ul style="list-style-type: none"> • The panel feels that most of the building heights are appropriate, noting that 6-8 storeys appears to be reasonable in this location. • However, it feels that the 19-storey landmark building is too tall and would like to see consideration of whether this can be reduced. It raises concerns regarding the quality of life at the higher storeys, as well as the experience at ground level for pedestrians, noting that a building of this height could be very oppressive. It notes that there are other ways of making a building a landmark besides height. • The panel notes the huge increase in density that this development will bring to the area. It would like to see consideration of the impacts on existing social infrastructure and how this could be addressed, for instance through Section 106 contributions. 	<p>The 19-storey ‘marker’ building has always been identified as the location of the tallest building on site. The total number of storeys is less than that previously approved on this site and is within the consented heights under the parameter plans.</p>
<p><u>Public realm, amenity and landscape</u></p> <ul style="list-style-type: none"> • The panel commends the approach to landscape design and feels that this element of the masterplan is very successful. • It questions the extent of hard landscaping and would like to see further detail of the private courtyard and podium areas that provide more soft landscaping. • Given the number of family-sized apartments proposed, the panel would like to see further detail and opportunities for unsupervised play, alongside features integrated within the public realm and streetscape. • The panel feels that the art installation proposed would be a good opportunity for the community to interact with. The 	<p>Noted. These matters will be further assessed below.</p>

<p>precedent of Jacques Rival's bird cage swing – IFO (Identified Flying Object) at Kings Cross – was noted as an installation that appeals to all ages and brings an element of fun to public space.</p> <ul style="list-style-type: none"> • The panel welcomes the design of the central square, noting that there are very few spaces of this size and nature within the local area. It feels that a water feature would be well-used and popular, noting the Granary Square fountains as a good example. • The future management of the public spaces will be crucial, and the panel would like to understand whether the council or Berkeley Homes will be responsible for the maintenance of these areas. • The possible inclusion of allotments within the private courtyards is welcomed, and the panel sees this as a great opportunity for residents living in the flats to have access to growing space. • However, it stresses that these opportunities need to be communicated explicitly to residents given that some areas will be actively managed, and other areas given back to the community. • The panel has concerns that the green roofs proposed to the taller buildings will be challenging and would like to see more detail to understand how these spaces will work; particularly to ensure the planting specified can thrive at these heights given environmental considerations such as wind and exposure. • It also notes that pigeons could be a problem, for both the buildings and public spaces, and that a strategy for this should be adopted early in the design to ensure that maintenance and cleaning is addressed. • Although this phase does not directly abut the canal, the panel recommends that the design team consult with the Canal and River Trust, given the relationship with the Paddington arm of the Grand Union Canal for the latter phases of the masterplan. It notes that this stretch is very well-used and would like to see more detail of the site's relationship to the towpath, as well as opportunities for activities along the waterfront. 	
<p><u>Ground floor uses</u></p> <ul style="list-style-type: none"> • The panel would like to see further consideration of the retail units, to ensure that local, independent businesses are represented. It notes that this is a key characteristic of Southall that should be included in this scheme. • The proposal for a larger food store seems appropriate, and the panel suggests that this will be positive by making this community offer easily walkable, avoiding reliance on car journeys. • The panel would like to see consideration of 'necessity shops', such as chemists and basic home goods, and suggests these uses could help drive footfall through the retail streets. 	<p>Noted. Council Officers and Berkeley are working together to ensure the most appropriate mix of retailers are available on site. However, Council cannot dictate a requirement for local, independent businesses.</p>
<p><u>Tenure and affordability</u></p> <ul style="list-style-type: none"> • There were some concerns about the potential negative impacts of buy-to-let properties and the panel would welcome further detail of the tenure mix. • The panel welcomes the number of affordable units delivered by the scheme. However, it is concerned that the scheme doesn't seem to be designed for the local need in Southall. 	<p>Agreement has been reached to provide affordable workspace as part of this application.</p>

<ul style="list-style-type: none"> • The panel would encourage the design team to consider and consult with the existing community to understand what housing is needed for the local population. • The panel also feels that the affordable housing is currently too segregated and that this should be more evenly distributed across the different blocks. 	
<p><u>Community benefit</u></p> <ul style="list-style-type: none"> • The panel welcomes the proposal to bring the health centre forward as part of this phase. It would like to see further detail of how this will be operated, for instance by the NHS or a private provider. • It appreciates that a school is planned for the later phases of the masterplan, but would like the applicant team to consider whether this could also be brought forward, to meet existing local need and accommodate the number of family homes delivered in this phase. • The panel would like to understand the key driver for the development, questioning whether social infrastructure or landscape is the main priority for the scheme. It recognises that both are valuable community assets that support health and wellbeing, but it has concerns that the existing social infrastructure will be under a lot of pressure and this should be addressed. • The offer of public toilets is welcomed by the panel. 	<p>Noted.</p>
<p><u>Parking, servicing and refuse</u></p> <ul style="list-style-type: none"> • The panel notes the amount of car parking is less than 1-space per dwelling. It would like clarity on how the spaces will be allocated, to ensure that the distribution of these is done fairly across the different tenures and unit types. • The refuse and recycling strategy should be considered further. The panel would like to ensure that the public spaces are kept clean and tidy, for both the residents and people visiting the area. The placement and number of bins, as well as collection routines, should be given full consideration. 	<p>Parking remains covered by the Outline Consent. The applicants have proposed parking that is well below the 0.7 spaces per unit allowed under the Outline Consent. Refuse will be collected from the waste storage areas in accordance with LBE's waste management guidelines.</p>
<p><u>Sustainability</u></p> <ul style="list-style-type: none"> • Given the original application was approved in 2010, the panel recognises that current targets may not be applicable. However, given that understanding of climate impacts has moved on significantly in the last 10 years, the panel finds this approach disappointing. • The panel urges the design team to address the latest guidance and changes in policy, to better respond to Ealing's target that all developments be carbon neutral by 2030. • It would also like to see more detail of the heating and ventilation strategy for the apartments and commercial units. 	<p>Councils Energy Officer has reviewed the Energy Strategy and confirmed it is generally supported subject to a carbon offset payment and conditions of consent.</p>

Officers Response:

The matters raised by the Design Review Panel (DRP) in first meeting (13/07/2021) were responded to by the applicant and the DRP as part of the second meeting of the DRP panel (dated 07/12/2021). Therefore, the second set of DRP comments will be addressed as part of the assessment below.

The Community Review Panels comments will be taken into consideration within the assessment below. However, it is noted that comments relating to S106 contributions and sustainability measures are separately covered by the Outline Consent and are not specifically matters for consideration under this Reserved Matters application.

REASONED JUSTIFICATION PLANNING CONSIDERATIONS**Planning Assessment:**

Development proposals are assessed in terms of their potential impact on the local and wider environment, on the amenities of the occupiers of surrounding development, taking into account the relevant development plan policies for the area and all other material planning considerations.

As a reserved matters application, the main issues for consideration are:

- Urban Design comprising:
 - o Layout;
 - o Scale;
 - o Appearance;
 - o Landscape;
- Impact on amenity of neighbouring properties;
- Standard of Accommodation;
- Affordable housing;
- Highways, transport and parking;
- Environmental Health;
- Energy and Sustainability; and
- Section 106 and Community Infrastructure Levy.

Preamble

The Outline Masterplan for former Southall Gasworks site was approved in September 2010 (as per planning permission P/2008/3981). An application to vary Condition 3 of Planning Permission P/2008/3981 to revise the approved outline masterplan for the site was submitted to the Council in August 2015 and subsequently granted planning permission subject to a deed of variation of the S.106 in February 2016.

Since February 2016, there have been another three revisions (not including non-material amendments) to the approved site masterplan. These subsequent revisions have retained the key characteristics of the masterplan including the eight (8) 'Character Areas' which are:

- The Eastern Gateway;
- The Emporium;
- Central Park;
- The Northern Quarter;
- Southall Mills;
- Minet Waterside;
- The Canalside; and
- The Western Gateway.

This proposal represents the sixth reserved matters application at The Green Quarter. Previous permission has been granted for Phase 1 (160115REM), Phase 2 (185158REM), the Spine Road (172953REM), the Linear Park (195279REM) and Phase 3 (205101REM).

The approved and partially completed Phases 1 and 3 (formerly Phases A and C) have delivered the first elements of the Northern Quarter which is a residential precinct that seeks to integrate the existing urban fabric with the proposed streets of The Green Quarter.

Phase 2 will deliver a vibrant mixed-used quarter centred on a Town Square. It will contain a mix of building uses, including a cinema, retail uses, office space and restaurants and will be the principal focus for community events and festivals.

The proposal has been assessed having regard to the Outline Masterplan and all relevant planning

policy and guidance. Policies D1, D2, D3, D4, D5, D6, D7, D8, D9, D14, H1, H6, H7, H10, G5, SI1, SI2, T1, T4, T5 and T6.1 of the London Plan (2021), policies 1.1, 1.2, 2.1, 2.8, 5.3, 5.4 and 5.5 of the adopted Ealing Development (or Core) Strategy 2026 DPD; policies 5.10, 5.11, 5.12, 7A, 7.3, 7.4, 7B, 7.7, 7D of the adopted Development Management DPD and Policy SOU5 of the adopted Development Sites DPD and the NPPF are the relevant development plan policies to assess the urban design merits of this reserved matters scheme.

Urban Design

Layout

Layout Principles

Phase 2 is the gateway to the Southall Green Quarter and is located at the eastern edge of the wider Overall Site bordering the rear boundaries of the existing properties on Grange Road and Lewis Road to the north, the Water Tower and associated cottages to the east, the Great Western Main Line to the south and the National Grid Retained Land to the west.

Phase 2 forms a crucial new commercial and residential centre within the Green Quarter and with the completion of Phase 1, creates a significant portion of the new neighbourhood on the eastern side of the site. The applicant's aspiration for Phase 2 is to create a vibrant, creative commercial centre, supported by high quality new residential neighbourhoods, and a series of exemplary public spaces activated by offices, shops, cafés and restaurants.

This Reserved Matters application shares the same principles defined by the Outline Consent. However, this application seeks to make the following changes to the previous Reserved Matters approval:

1. Introduce residential uses to buildings E and F (previously hotel, and office use), with building footprints to reflect the new use
2. Create a landmark building fronting Arrival Square
3. Create a landmark building fronting Town Square and Central Gardens
4. Re-configure the buildings fronting Central Gardens to allow more permeability through to Central Gardens
5. Reduce east-west axis buildings to reduce the number of north facing units
6. Reduce the footprint of large blocks (more buildings with smaller footprints)
7. Define a strong architectural character which relates to the more civic nature around the Town Square (Quarter Yard)
8. Define an architectural character which relates to the softer landscaping of Central Gardens (Parkside) which is distinct yet shares a common thread with that of the Town Square

Similar to the previous Reserved Matters application (185158REM) the eastern portion of the site comprises a mixed-use, commercial and residential part of the site. The eastern part of Phase 2 comprises a predominantly residential part of the scheme.

Figures 1 illustrates the site location of Phase 2 within the wider Southall Green Quarter site. Figures 2 and 3 shows the location of buildings (relevant to this specific application) and layout principles for this Phase based on land use.

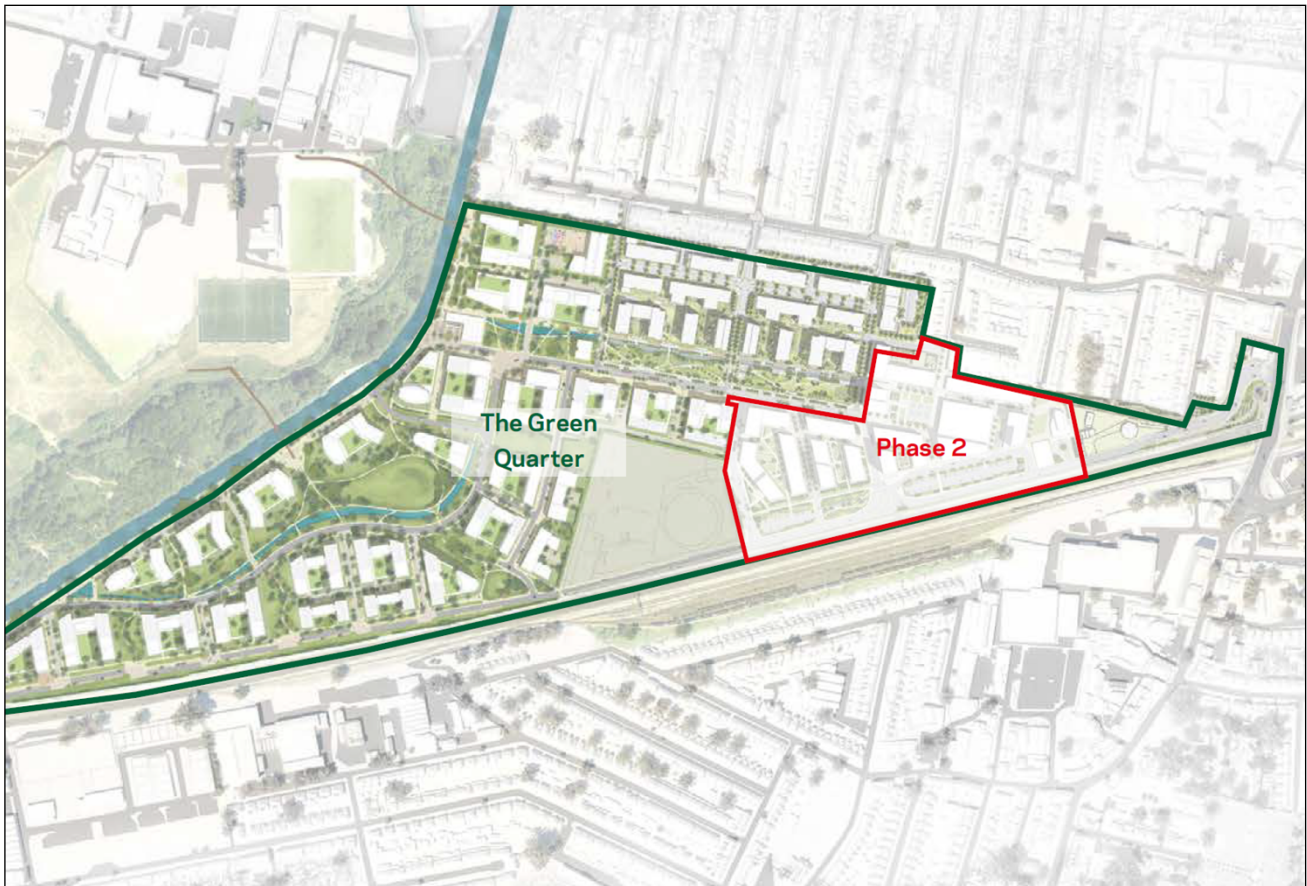


Figure 1: Location and Layout of Phase 2 within the overall Masterplan



Figure 2: Application Buildings

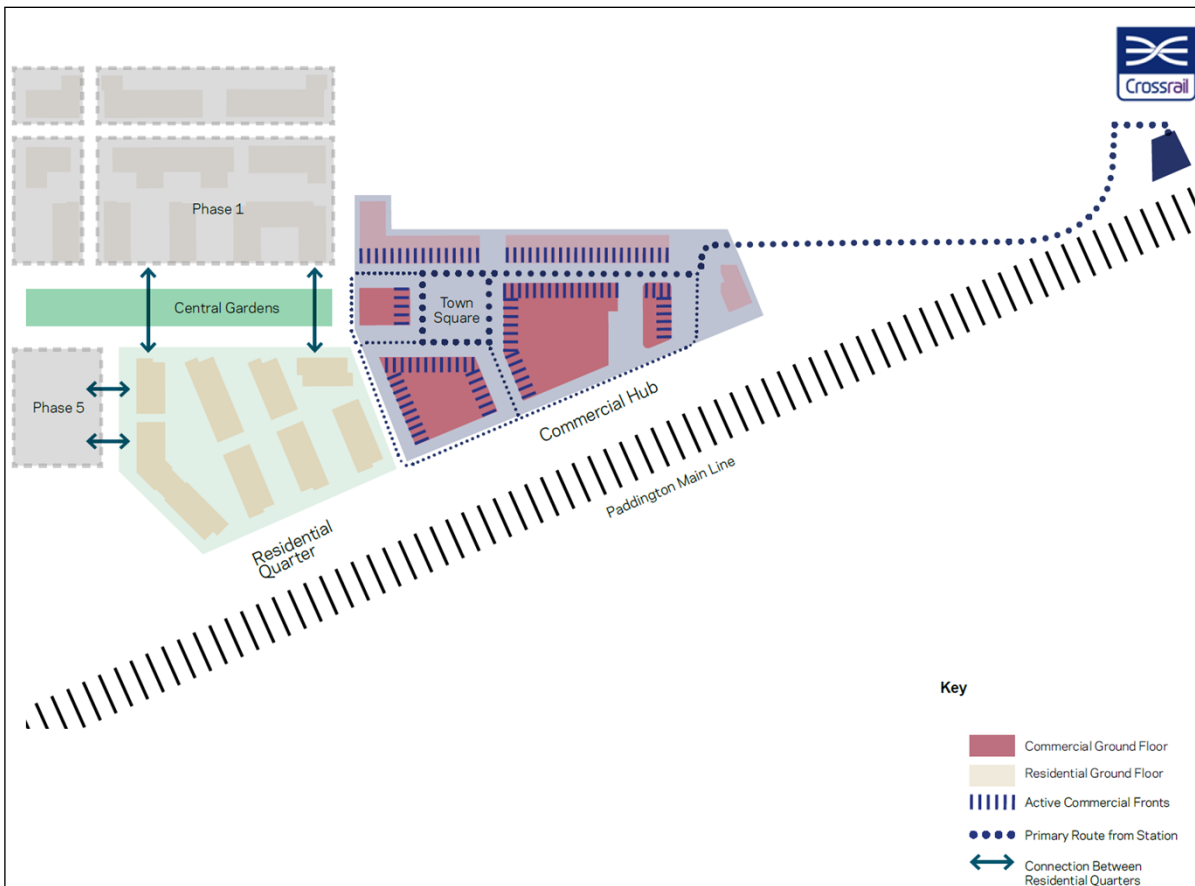


Figure 3: Ground Floor Uses and Connections

Building Arrangement

The design proposals for the Phase 2 layout are based on the ‘Placemaking’ and urban design principles established in the approved masterplan (Outline Consent). Building positioning and orientation are defined by the existing constraints of the site, the existing buildings to the north, new public spaces and streets.

Orthogonal buildings and streets are proposed to the north of the site to sensitively respond to the existing urban grain adjacent and around the Town Square. At the heart of Phase 2 where the Central Park meets the Town Square, a marker building is proposed, linking the two public spaces and acting as a focal point from the east and the west.

The diagonal Cross Streets connect The Straight and the Spine Road, and these determine the building footprints and private amenity spaces to the west and the south of the site. Each building is carefully designed to ensure active frontages open onto each street, with own door access homes at the street level for some buildings.

There are a total of seventeen buildings proposed on the Site. Figure 2 illustrates the building arrangement, including the three buildings excluded from this application. Figure 3 shows the proposed building arrangement in relation to the connections available within the wider area.

Building uses – Commercial

The revised Reserved Matters application represents a reduction in the overall commercial floorspace when compared to the previously approved scheme for the site. It remains within the maximum quantum of commercial floorspace that was approved under the Outline Consent.

The main changes since the 2018 Reserved Matters approval are:

- Removal of the hotel from this Phase (as accepted in the 2020 NMA) reflecting the oversupply in the local market and the particular impact of a reduction in Heathrow passengers which leaves numbers still 75% down on pre-Covid levels with associated impacts on occupancy meaning it is not realistic to bring this forwards in the short/medium-term;
- Reducing the quantum of retail space, in particular non-food retail space, reflecting the shift in spending towards online channels which now account for over 25% of overall retail spend and are forecast to grow further, and consequences on the sector with multiple retailers reducing their physical footprint and consolidating towards higher order centres. Nonetheless a 847 sq.m foodstore and a further 2,665 sq.m of non-food retail floorspace which is more than enough to create a vibrant place with active ground floor frontages and animated public realm;
- Reducing the size of the cinema reflecting the oversupply of cinema screens in the locality, the notable impact of Covid on admissions which are running 45% down, and impacts of the growth of streaming platforms. A small boutique cinema provision of 736 sq.m is proposed which will contribute to the scheme’s leisure offer and that of Southall more widely.

Table 8, below, shows the comparison of commercial floorspace approved for the site and proposed within this application.

Table 7: Commercial Floorspace Summary

Phase 2: Commercial Floorspace Summary						
Unit Type	Outline Consent (171562VAR)		180858REM		216077REM (+ excluded Buildings B, C & D)	
	<i>sqm</i>	<i>Sq/ft</i>	<i>sqm</i>	<i>Sq/ft</i>	<i>Sqm (% change)</i>	<i>Sq/ft</i>
Non-Food Retail E(a)	14,200	152,847	3,544	38,147	3,512 (-17.8%)	37,802
Food Retail	5,850	62,969	732	7,879		
Class A3-A5 uses	1,750	18,837	1,747	18,805	1,701 (-2.6%)	18,305
Hotel	9,650	103,872	5,987	64,443	-	-
Conference and Banqueting	3,000	32,292	-	-	-	-
Cinema	4,700	50,590	2,221	23,907	736 (-66.8%)	7,922
Health Care Facilities	2,550	27,448	-	-	1,028	11,065
Education Facilities	3,450	37,135	-	-	-	-
Office/studio units	3,500	37,674	1,495	16,092	2,419 (61.8%)	26,034
Sports pavilion	390	4,198	-	-	-	-
Energy centre	1,885	20,290	-	-	-	-
TOTAL	50,925sqm	548,152sq/ft	15,726sqm	169,273sq/ft	9,396sqm	101,128sq/ft
Residents and Estate Management Facilities (Not included)	N/A	N/A	2,080sqm	22,388sq/ft	2,028sqm	21,827sq/ft

To justify the loss of commercial floorspace within this application, the applicants submitted additional consultants' reports including:

- Economic Assessment Report prepared by Icen; and
- Commercial Demand Assessment prepared by AND.

These reports put forward the argument that the proposal was both policy compliant within the parameters, which it is, as well as providing the maximum amount of commercial floorspace which is viable on the site.

It is noted that a previous non-material amendment (204493NMA) approved the amendment to the parameter plans – altering the required mix of uses within the Town Centre and removing the requirement for a hotel and a large multiplex cinema. In this regard, the current proposed mix of uses is compliant with the parameter plans.

In order to properly assess the applicants proposal, a Commercial Demand Assessment Review was undertaken by external consultants (CAG) so as to review the reports and documentation prepared by the applicants. CAG were engaged by Ealing to undertake this review as an impartial third party. This report provided the following conclusions:

- The proposed commercial floorspace set out in the Icen report appears to represent a large reduction from 17,806sqm in the outstanding consent to 9,395 sqm (being inclusive of Buildings B, C and D) in the current proposal – a reduction of nearly half.
- The reduction is accounted for by three elements of the original consent:
 - o 5,987sqm for the hotel
 - o 1,485sqm for the reduction in the size of the cinema
 - o 2,080sqm for the estate management and resident facilities
- In employment terms the proposed increase in office floorspace will more than offset the reduction in cinema space, which is a low density use.
- A large hotel with conference and other facilities would have generated a significant number of jobs but without an operator to take it on it is not a viable proposition and we accept the argument that in the short-medium term there is unlikely to be demand for such a product in Southall.
- The issue then becomes whether the land initially allocated for a hotel should be reserved for some other form of commercial development to come forward in the future.
- It is hard to quantify precisely how much commercial workspace is appropriate, but in broad terms we think the AND and Icen reports have presented a reasonable assessment of commercial market demand for Southall Green Quarter at the present time.
- Our advice is that the securing the right type of commercial space at the right price will have a more beneficial effect for the Southall economy than trying to increase the quantum. There will be demand for workspace in Southall, but it will be for low-cost workspace suited to micro-businesses, start-ups and local entrepreneurs.
- Securing support from the developer for affordable workspace as a response to the overall reduction in quantum of commercial floorspace may be something worth considering.

The Local Planning Authority recognises that the proposed reduction in commercial floorspace within Phase 2 has been well considered by the applicant and has been supported by robust evidence. In order to offset this loss through an appropriate public benefit, the applicant has agreed to provide 'Affordable Workspace' within phase 2, which would be provided at discounted rents for start ups and small businesses within the area. Details of this provision would be set out in an Affordable Workspace Plan, that will be submitted to for approval in writing by the Local Planning Authority before more than 500sqm of the commercial floorspace within Phase 2 has been occupied. This requirement would be captured within a deed of variation to the original S106 agreement.

Overall, having regard to the above, the proposed reduction in commercial floorspace is considered to be acceptable in this circumstance, given the robust evidence provided and the agreement to

offset this loss through the provision of affordable workspace that would benefit small start up businesses in the area.

Building Uses - Residential

The building uses of the Site move from mixed use buildings in the east to fully residential in the west. Buildings M – U sit to the west of the Town Centre and will be fully residential.

This is in line with the previously approved use of this site and the buildings would be laid out and designed in a similar manner to the residential buildings approved in Phases 1 & 3.

Vehicular and Pedestrian Routes

There are a number of vehicular and pedestrian routes proposed for the Site, making the proposed development permeable and easy to navigate for pedestrians and vehicles alike. These have been designed to establish a clear hierarchy between primary and secondary routes.

Two primary east-west vehicular routes are created, which run through the Site and connect the wider Masterplan Site to the west with Southall Station to the east. These routes originate at the eastern boundary of the site at the Eastern Gateway before branching off into two routes from a mini roundabout located midway through the Site, on The Straight.

Two secondary vehicular north-south residential 'Cross Streets' have been created on the Site, connecting The Straight and the Spine Road between buildings L and O, and between buildings Q, U, T and the Cadent Gas Retained Land.

A primary pedestrian route through the site has been created connecting from Southall Railway Station, through the Eastern Gateway with the Central Park, passing through the Arrival Square and Town Square. Two secondary pedestrian streets connect the Town Square and The Straight between buildings E and H as well as between buildings I and K.

Open Spaces, Landscaping & Access

Two major public spaces would be created within Phase 2 - The Arrival Square at the eastern entrance of the site and the Town Square located at the heart of the site with direct visual and physical connections to the Central Park to the west.

The buildings would be arranged as courtyard blocks facing streets to create a series of rear private shared amenity spaces for the residents. Two generous landscaped courtyards at grade for buildings M-P and Q-U and two raised podium courtyards located on the first floor for buildings within the Town Centre.

Above the utilities corridor and gas easement a large linear visual amenity space is created along The Straight and has been designed to ensure it acts as an intense biodiverse green corridor. The amount of grade car parking has been reduced on this space to increase the amount of open space and planting.

Overall

Overall, the proposed layout is generally consistent with the approved Outline Masterplan as well as the existing Reserved Matters approval for Phase 2. It will continue to interconnect well with Buildings B, C and D (that sit outside of this application) as well as the approved schemes at Phases 1 and 3. As such, the layout is considered to be acceptable.

Scale

The Phase 2 site offers a significant opportunity within the Southall Green Quarter masterplan site to create a new mixed-use centre. However, the entrance to the site from the east is constrained and the development itself must negotiate a shift in character and scale, from the low-rise residential streets to the north, to the railway line and industrial buildings to the south. In addition, CAA height restrictions limit overall building heights across the site and the gas easement running parallel to the railway line along the southern boundary also restricts the building footprints and positioning along this edge. The Outline Consent reflects these constraints and this Reserved Matters proposal sits within its parameters.

The proposed height and scale re-enforces the following key principles that underpinned the previous Reserved Matters approval for Phase 2:

- A tall landmark building at the heart of the scheme that relates to both the Town Square and Central Gardens
- Taller buildings fronting the railway creating a strong rhythm of higher buildings
- Shorter buildings between the taller elements to help define rhythm
- Stronger north south orientation of buildings to avoid north facing units and allow visual permeability through to Central Gardens

The current Phase 2 proposal avoids making any changes to Buildings B, C and D. These sites continue to remain at an appropriate scale that have been designed to respect and respond to the existing scale and massing of the 2 storey residential buildings to the north and the collection of buildings, including the 7 storey Water Tower, located between the Eastern Gateway and the Phase 2 site.

Within the Phase 2 site, there would be a mix of building heights from 7 storeys up to 19 storeys in the centre of the site between the Town Square and the Central Park. Along the Southern Fringe adjacent to the railway the residential buildings rise from 7 to 13 storeys creating four taller buildings that are intermixed between shorter buildings to help define rhythm.

The maximum and minimum height parameters for buildings are set by the approved Outline Consent plot vertical parameter plan approved in 2017. The proposals for phase 2 would comply with the height parameters across the development.

The maximum and minimum height parameters for buildings are set by the approved Outline Masterplan plot vertical parameter plan approved in 2017. All proposed building structures would sit below the maximum heights established within the parameter plans. The maximum heights set out in the parameter plans are listed in Figure 4 below. The compliance of the buildings is illustrated in Figure 5.

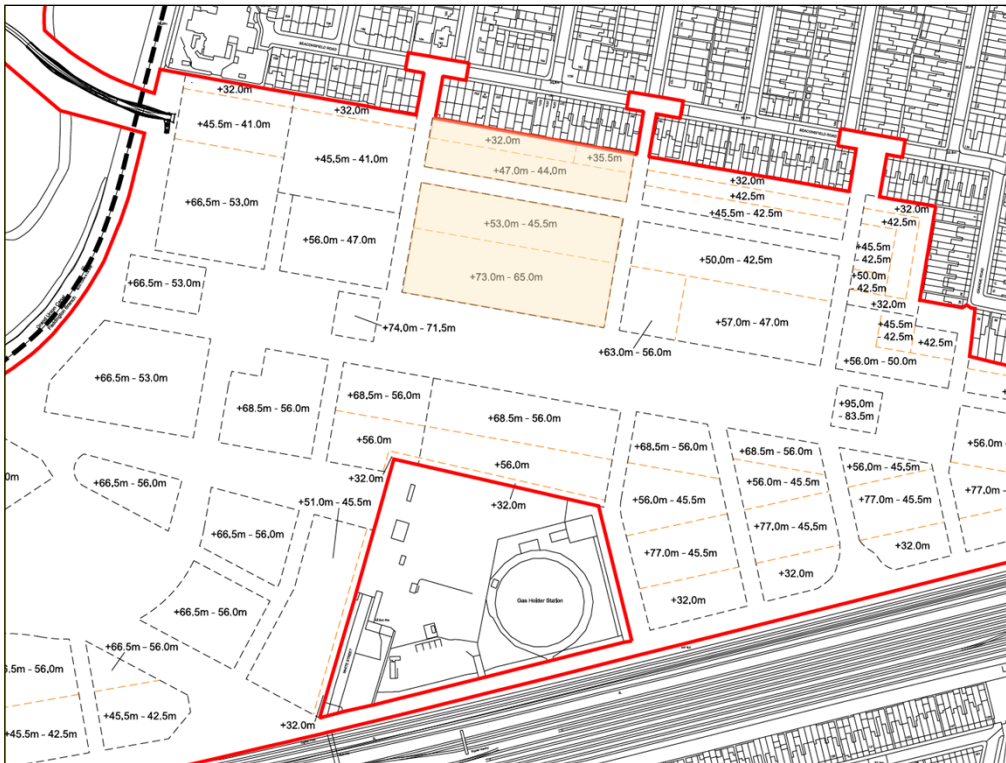


Figure 4: Consented Vertical Parameter Heights



Figure 5: Compliance with Parameter Plans

The scale of the development would be respectful to and respect the sensitive edges at the northern and eastern boundaries of the site adjoining Grange Road, Lewis Road and Phase 1. The buildings would progressively get larger to the middle of the site, with the tallest buildings located in the central part of the site at the western end. Figure 5, below, illustrate this height progression across the Phase 2 site.

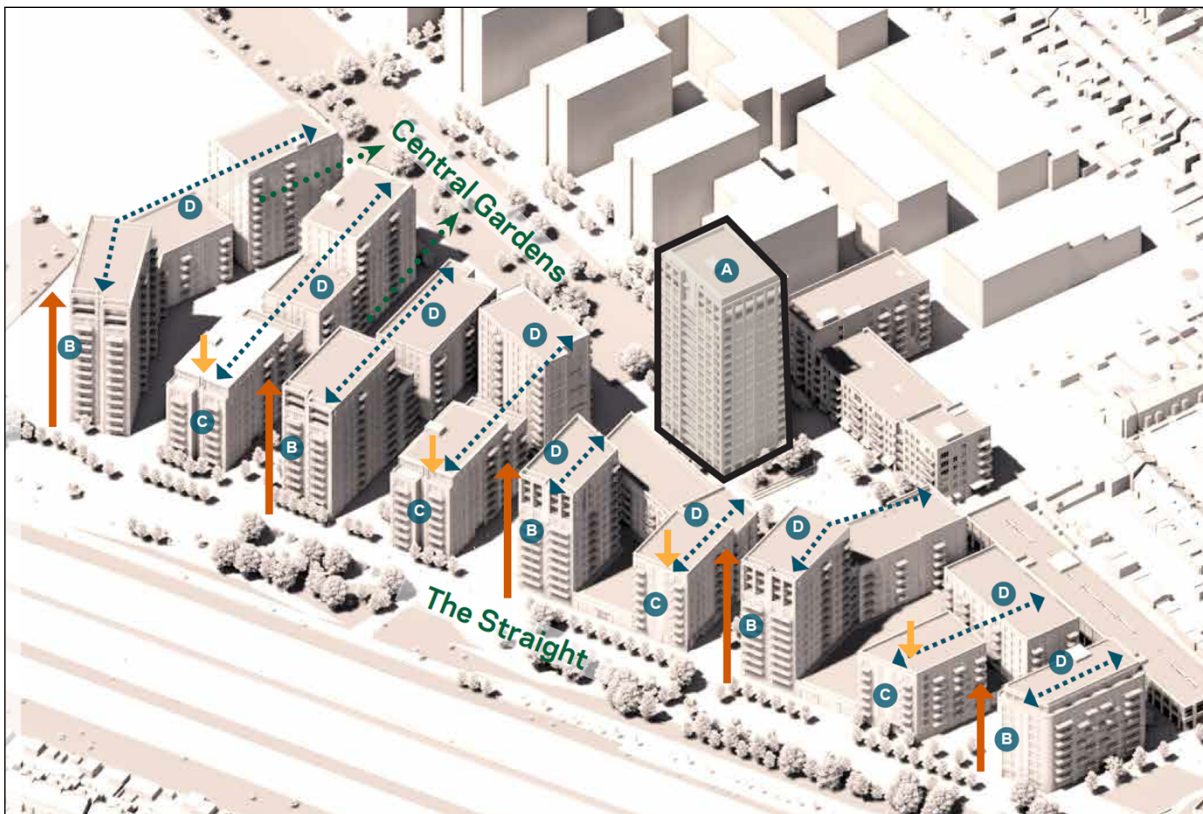


Figure 6: Proposed Building Massing

Overall, the scale of the built form has had regard to the context of the site, the Outline Masterplan, and the parameter plans. It would accord with the parameter plans and the current development plan policies and, as such, is considered to be acceptable.

Appearance

Design Concept & Character Areas

Phase 2 is shaped both by the smaller-scale neighbouring domestic residential buildings and the historic large-scale, industrial infrastructure which once stood on the site, and still exists to the south of the site. These contrasting influences are expressed through the material palette across the entire Phase 2 site, both within the architectural components and the landscape.
 Industrial Influences

To align with the wider masterplan character areas, Phase 2 is divided into 2 distinct areas: Quarter Yard and Parkside. A marker building acts as a unique landmark whilst bridging the gap between the two.

The Quarter Yard is located at the entrance to the Phase 2 site when approached from the centre of Southall or the train station. Within the masterplan this area is defined by strong but simple forms, special tops and a robust framed repetition.

The Parkside character area is located to the Western edge of Phase 2 and borders both the railway and the Central Gardens. This area is defined by a consistent rhythm and a ordered backdrop.

The Marker Building is located at the centre of the Phase 2 development and is to be the focal point of the development.

The plot holds a prominent location on the boundary of the Central Gardens and the Town Square. Within the masterplan this building is prescribed to be a landmark building of the same gravitas as the existing Southall water tower.

The location of the character areas is illustrated within Figure 7 below.

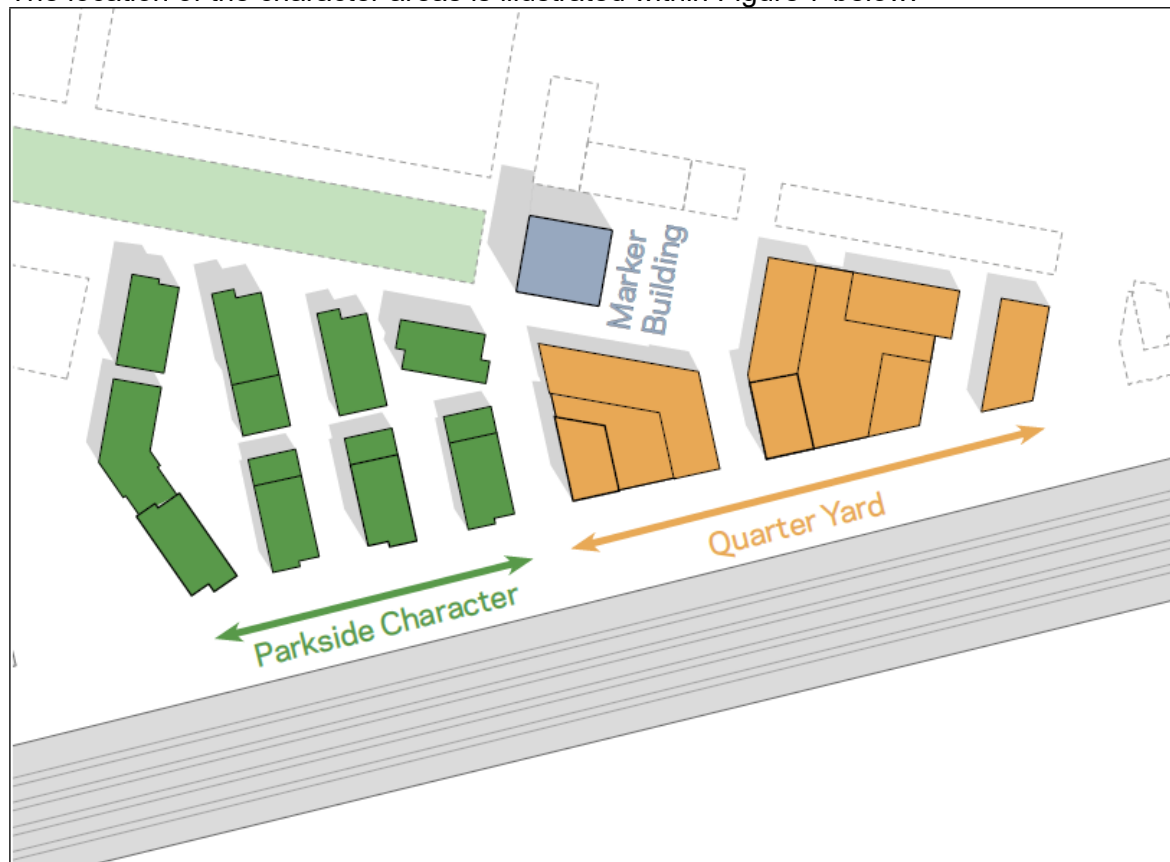


Figure 7: Proposed Building Massing

Façade Composition and Design

In general, the proposed facades have been composed and design based on the location of each building within a specific character area. The 3 characters areas of the Phase 2 development are designed to work harmoniously alongside each other whilst still providing a unique feel to the surrounding public spaces. The design of the character areas follow simple rules specific to the character references defined within the wider masterplan.

The facades within the Quarter Yard have been primarily defined using a strict vertical grid which provides a clear rhythm to the facade. This rhythm combined with an appropriate fenestration tie the Quarter Yard to the site’s historical industrial heritage.

The buildings within the Parkside area are to be designed to provide a soft backdrop and link between the industrial heritage to the East and future residential buildings to the West, while the varying facade treatments can allow a scale of visual importance. The buildings also reference the existing and consented buildings of Phases 1 and 2.

The facades within Parkside are primarily defined using a strong horizontal banding which helps to reduce the buildings down into clear layers. The buildings differ in their use of this horizontal banding, from a 2-storey rhythm to a single storey rhythm.

The breakdown and composition for each character area is illustrated within Figure 8 below.

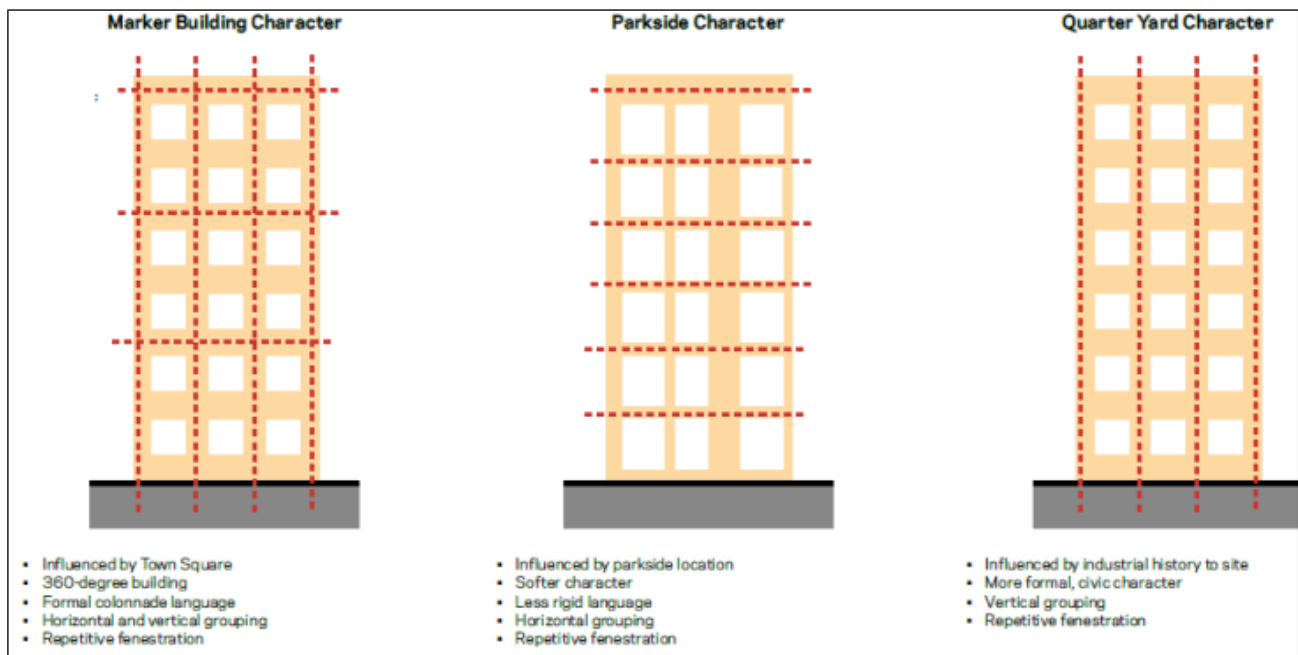


Figure 8: Façade Composition and Character – by Character Area

Marker Building – Building A

The tallest building proposed on Phase 2 is Building A which would be 19 storeys. Building A would be located at the important junction between the Town Square and the Central Gardens and as such would be identified as a ‘marker building’ in the facade strategies. The tall vertical nature of the building is emphasised by breaking the building into two separate vertical volumes, establishing a top, middle and basement and then creating vertical piers, helping to reduce the mass of the building. The massing provides a slender proportion towards the east and west, accentuated by the vertical emphasis of the facades and the lighter treatment of the top floors. Deep incisions along the longer facades create a hierarchy in the composition.

The colonnade wrapping around the entire ground floor provides a civic character to the base of the building. The building features primarily white brick, with black brick and black metal. These materials chosen for the external finishes will further enhance the distinctiveness of the building and will set it apart from the rest of the blocks, whilst respecting the strong common character to the entire development.

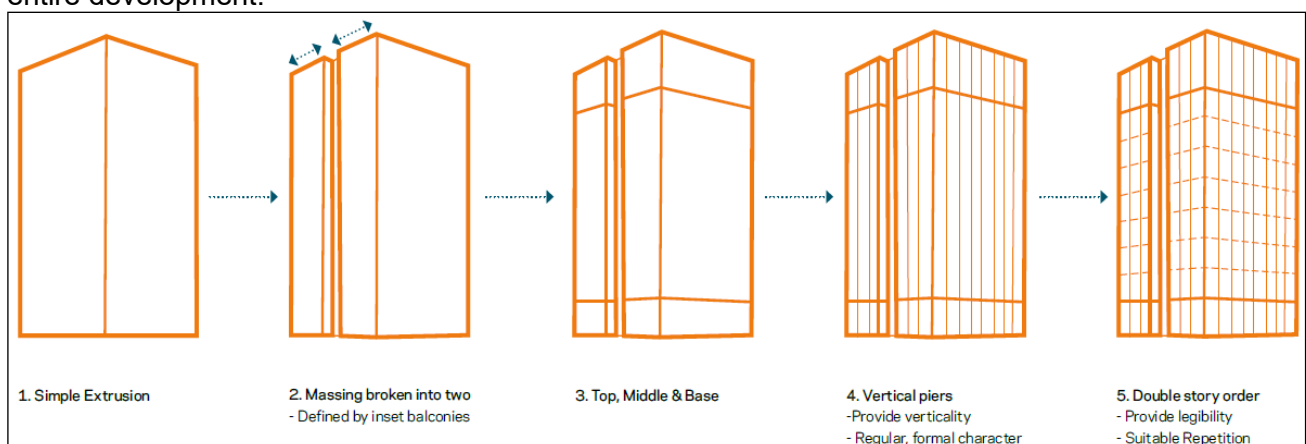


Figure 9: Design Rationale of Building A

Arrival Square - Building E

Situated at the entrance to the site, building E is a gateway building and as such the architecture has been developed to create a suitable entrance. The unique rounded corners of the building removes any unwanted sharp angles and allows the facade to read as one single plane, whilst also relating to the Water Tower.

The materials for Building E are in keeping with the industrial heritage of the Quarter Yard. Primarily built with red brick, the facade is divided further with the use of a black brick base and a setback metal top.



Figure 10: Building E

Building F

Situated on the corner of the Town Square, this building provides one of the corner stones of the block. In order to place emphasis on the Town Square and reduce the scale of the facade the building has been suitable broken down into two clear parts. It features full height windows and strong parapet detailing, while the longer portion of the facade uses a traditional window height and a softer parapet. The two primary sections of the facade are separated from each other with the use of a setback.

The materials for building F conform to the industrial heritage of the Quarter Yard. The building is built primarily of red brick, with soldier course detailing and red stonework used to distinguish the base, middle and top.

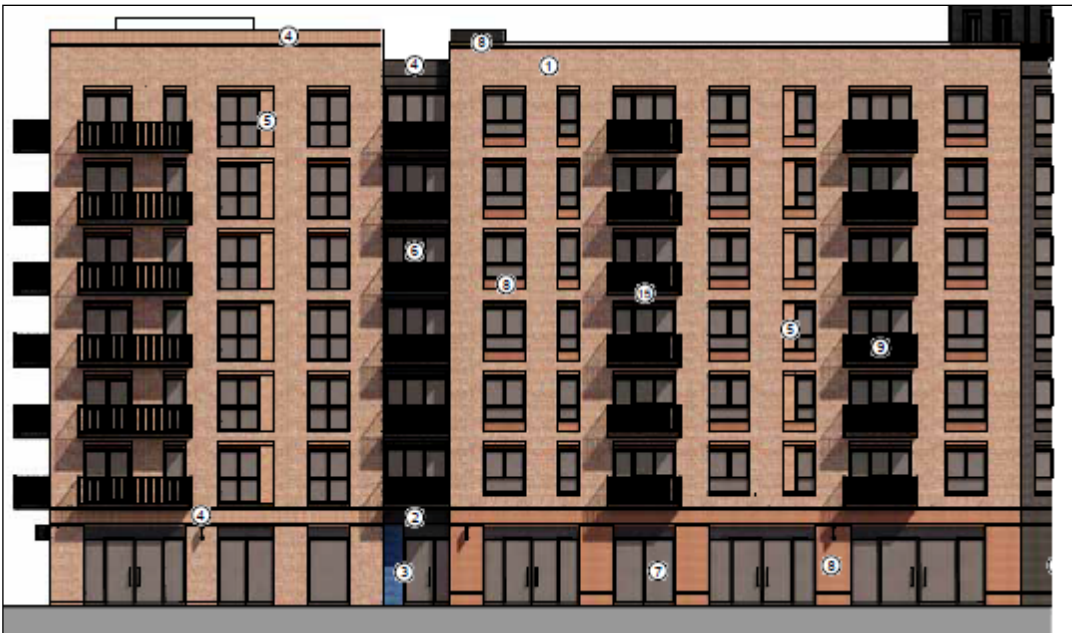


Figure 11: Building F

Building G

Located along the main pedestrian access route, building G has been designed to provide suitable backdrop to the proposed commercial uses of this street. The building features a clean vertical rhythm that wraps around all four sides. This building is designed to lack a primary facade to allow it to provide a suitable backdrop to building E.

The materials for building G maintain the industrial heritage of the Quarter Yard, with red brick providing the prominent material. The red brick is broken up with the use of red stonework to distinguish the base, middle and top orders.

Stonework is further used to provide window cill detailing, an element which is used to reduce the horizontal aspects of the building and promote the vertical rhythm of the facade.



Figure 12: Building G

Building H

Building H has been designed to provide a subtle backdrop to the neighbouring building E. The design features a clear vertical rhythm, repetitive balconies and strong parapet detailing.

The materials for building H have been carefully considered to work in their own right whilst also being sympathetic to its position within the site. The building is predominantly made up of red brick and features soldier course brick detailing to define base, middle and top. The vertical rhythm of the facade is enhanced with the use of red stonework below window cills and the use of setback red brick with dark mortar.



Figure 13: Building H

Building I

The building utilises a clear vertical rhythm whilst also having a defined base, middle and top. The top of building I features a two-storey setback that aides to reduce the massing of the building along the railway elevation whilst providing differentiation along the West and East elevations. The materials for building I have been chosen to provide contrast to the neighbouring Quarter Yard buildings, whilst still aligning with the industrial heritage. The buildings blue-grey brick tones has been chosen to pay particular homage to the existing gasholders on the site. The building also features suitable brick detailing and stonework to separate base, middle and top.



Figure 14: Building I

Buildings J & K

These two buildings are situated along the length of the Town Square. Similarly to building F, these blocks are designed to be a marker for the corner of the square. To achieve this Buildings J&K are clearly separated visually, with the corner of the buildings standing one storey higher than its neighbours. The corner building is also differentiated with the use of full height windows and a strong parapet detail.

The materials for buildings J&K have been chosen to continue this differential treatments, with two subtle red brick tones used to provide a clear visual difference. The alternating use of soldier course detailing and stonework allow the buildings to read as cohesive yet unique. The corner is separated from the rest of the facade using a metal groove.



Figure 15: Buildings J & K

Building L

Building L is designed to provide clear definition between itself and the neighbouring buildings. Designed in a similar approach to Building I, the use of a two storey setback to the top two floors reducing the impact of the massing from the railway and along the West and East facades.

The materials for building L have been chosen to stand out against the predominantly red backdrop of the Quarter Yard. The choice of grey brick continues to reference the industrial uses of the site whilst providing variation. The design utilises two tones of mortar to differentiate the base from the bulk of the massing.



Figure 16: Building L

Building M

As one of the smallest blocks within the Phase 2 development, Building M's architecture focuses upon providing a subtle background to the surrounding landmark buildings. The form and facade build-up of the block is simple yet suitable for its location, the building is broken down by the use of a two-storey horizontal banding rhythm.

The material choices for Building M are to aid the crossover between the Quarter Yard and the Parkside, in using red brick the building allows the two zones to form a more cohesive whole. The building is predominately red brick while soldier course brickwork and red stonework are used to differentiate between base, middle and top elements.

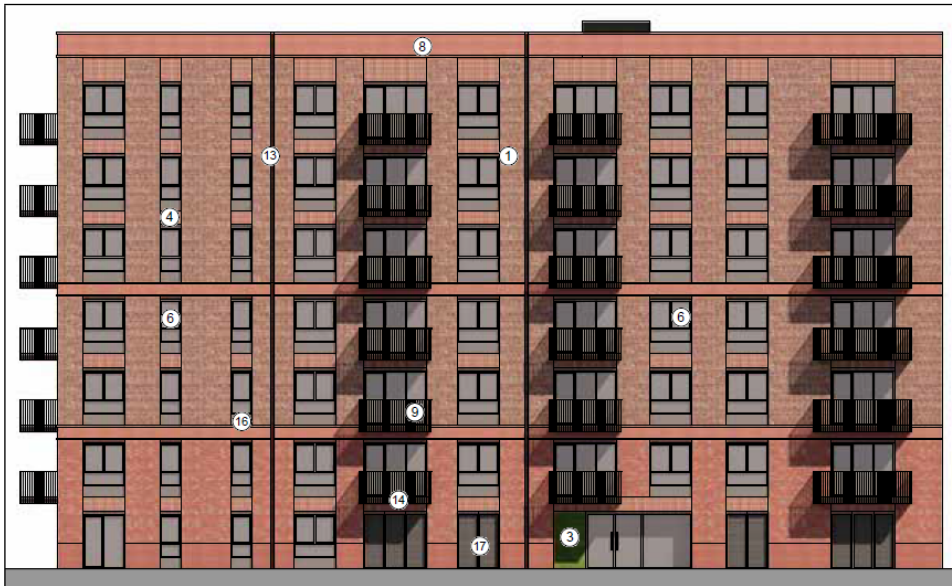


Figure 17: Building M

Building N

The building is located on the corner of both the Town Square and the Central Gardens. For a building in such a prominent location it requires multiple primary facades to address these public spaces. Uniquely to this building, these elevations are built up of a series of frames that reduce the scale of the elevation and create a unique rhythm. The combination of a vertical and horizontal grid allows this building to neatly tie into the neighbouring Building A. In form, Building N steps down to face the Central Gardens. The use of this two-sided setback on the park facing elevations helps to breakdown the massing of the building when viewed from the park.

The materials for Building N are chosen to be distinctive when viewed in comparison to the surrounding buildings, in particular that of Building A. Building N is made up primarily of grey brick work, with grey metal panelling, grey brick with dark mortar and soldier course brickwork being used to provide detail and rhythm to the facade.



Figure 18: Building N

Building O

The massing of Building O can be clearly split up into two parts dependent on building height. This split has been emphasised through the use of a 2 storey horizontal rhythm or a 1 storey rhythm to the lower element. This distinction allows the volume of the building to be broken down as you view its long elevations from an oblique angle. To aide in the reduction of massing further, the top floor has been recessed on the primary elevation.

The materials for Building O have been chosen to fit in with the subtle tones of the Parkside aesthetic. The building is primarily built up from buff brick tones, with darker mortar introduced to distinguish between the base and middle. Appropriately coloured stone banding is included to break down the building into horizontal bands. The setback floor is constructed from a dark grey metal in order to provide stark contrast to the floors beneath it.



Figure 19: Building O

Building P

Building P is broken down into two distinctive blocks based upon height, with the vertical grouping varying between these blocks to aid in the differentiation. Facing towards the railway, the building features a setback on the top two floors that creates a distinctive top to the building.

The materials for Building P have been chosen to subtly stand-out from the Parkside character surrounding it. The building primarily features grey brick, while at the base this is combined with dark mortar in order to create differentiation to the floors above. The building features horizontal banding made up of a thin grey stonework strips with brick soldier course sandwiched between. Between horizontal bands the windows are grouped together using a dark grey metal.



Figure 20: Building P

Building Q

This building has been designed to form a key part of the backdrop to the Central Gardens, while the park facing elevation has been designed as a primary elevation which gives the architecture a clear direction. The building is divided into a clear base, middle and top, with differing materials and the setback top floor providing the definition

The materials for Building Q have been chosen to provide the subtle backdrop that Parkside requires. The majority of the building is formed of buff brick with a light mortar, while the base uses a darker mortar to provide distinction. The building also features stonework as horizontal banding, soldier course brickwork to link fenestration and a dark metal recess floor.

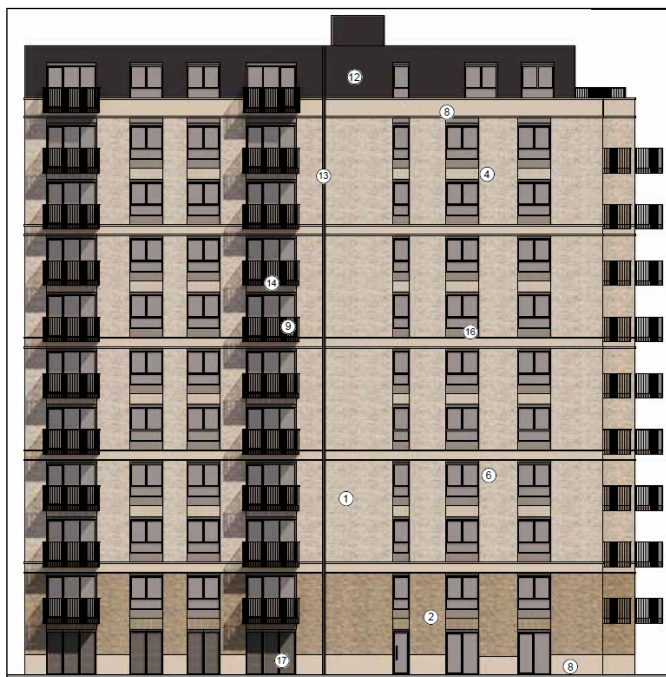


Figure 21: Building Q

Building R

Situated with a short elevation facing towards the Central Gardens, Building R has been designed to keep in with the Parkside typology. The building has a clear orientation with the primary facade facing onto the gardens, whilst the longest elevations have been reduced by a variation in block height and elevational treatment. The building also benefits from a clear base, middle and top.

The materials for Building R are chosen to allow the building to subtly become a part of the backdrop for some of the neighbouring buildings. The building primarily uses buff brick, while buff brick and a dark mortar is used for the base to provide a clear differentiation. The building is broken down by horizontal stonework banding that groups the building into single storey or 2 storey elements, between horizontal bands the windows are linked vertically using soldier course brickwork.



Figure 22: Building R

Building S

Located along the railway edge of the Parkside character area, Building S is designed with a clear direction to the facade with the primary elevation facing towards the site boundary. The building is formed of two distinct blocks formed of differing heights, which breaks down the long elevations of the building and provide variation when viewed from the street. The building benefits from a distinct top floor setback which further aides in reducing the massing of the proposal.

The materials for building S have been carefully chosen to allow the building to form part of the backdrop for the surrounding landmark buildings. The building is primarily formed of buff brick with a light mortar, while a buff brick with a dark mortar is utilised to provide a clear distinction for the base. The building features appropriately coloured stonework banding to create the horizontal banding of all the Parkside buildings. The recessed top floor is clad in a dark grey metal to provide clear contrast between the rest of the building.



Figure 23: Building S

Buildings U & T

The building is designed as a large local landmark building, defining the corner of the site. The building features two clear elements broken down by a difference of material and height. Both of these elements also feature a clear base, middle and top.

The building also features a clear direction, with the primary elevation facing towards the railway. The materials for buildings U&T are chosen to be suitable yet distinctive within the Parkside character area. The building is primarily formed from a unique dark buff brick, while this is combined with a dark mortar to provide a distinctive base and vertical grouping. The building also features soldier course horizontal banding which aides to break down the vertical massing of the building. The two varying elements of the building are separated by black brickwork in order to provide clear distinction between the two zones.



Figure 24: Buildings U & T

Summary

Overall, the proposed buildings appearance would be of a high-quality and standard and would present visually in a manner that is consistent with the existing and developing character of the site. The two character areas would be visually differentiated through the design and materiality to ensure that they are distinct areas within the wider site, achieving variety and enrichment, while establishing harmony without uniformity across the development.

Landscape

The Landscape and Public Realm approach for the overall site describes a journey from the vibrant urban streetscape of the town centre and Southall station (east of site) to a soft and naturalistic environment influenced by the character of the Minet Country Park (west of site). This transition is expressed by a proposed green infrastructure and a new pedestrian corridor that flows through the heart of the development, connecting to the Minet Country Park and the adjacent Grand Union Canal.

The landscape masterplan concept is based on a number of themes, which include:

- The flow of water from east to west. Bringing multiple benefits to life across the site.
- Creating a multi-functional Green Infrastructure.
- Establishing an integrated and holistic approach to biodiversity and wildlife through the creation of connective habitats with a significant biodiversity net gain.
- Developing a network of high quality and multi-functional open spaces and unlocking existing open spaces including the Minet Country Park.

- Improvements to health and well-being by creating walkable, playable, inclusive and engaging places for all.
- Increasing connectivity to the canal, adjacent open space, residential areas and the Minet Country Park.
- Ensuring the new community is permeable and accessible to the wider Southall community and natural surroundings.
- Improve access to the Grand Union Canal and Minet Country Park, to help reduce the area of deficiency in access to nature.

The London Wildlife Trust are strategic partners to Berkeley Homes and ecological advisors to the design team. Their role as a strategic partner demonstrates the aspirations of The Green Quarter to become exemplar in delivering biodiverse places for the benefit of people and wildlife.

Landscape Vision: Phase 2

The stated vision for Phase 2 seeks to deliver the strategy described as ‘The Flow’. The Flow is described as a new journey from East to West, from urban and formal, thru sub-urban, to rural and flowing. The flow underpins the masterplan building layouts and generates character through a series of parks and open spaces, flowing through links and connections across the site.

Landscape Strategy: Phase 2

Within Phase 2, landscaping would be distributed across the scheme, providing amenity and playspace for all residents of the development. The communal amenity space located in between the buildings would be for the use of residents only.

The stated strategy for the public realm is as follows:

- Green the street;
- Maximise green connections;
- Reflect parkland character into courtyards;
- Create planted courtyard gardens;
- Connect the courtyards;
- Integrate play; and
- Bio-diverse planting and habitat types.

Figure 25 below illustrates the proposed landscape masterplan.

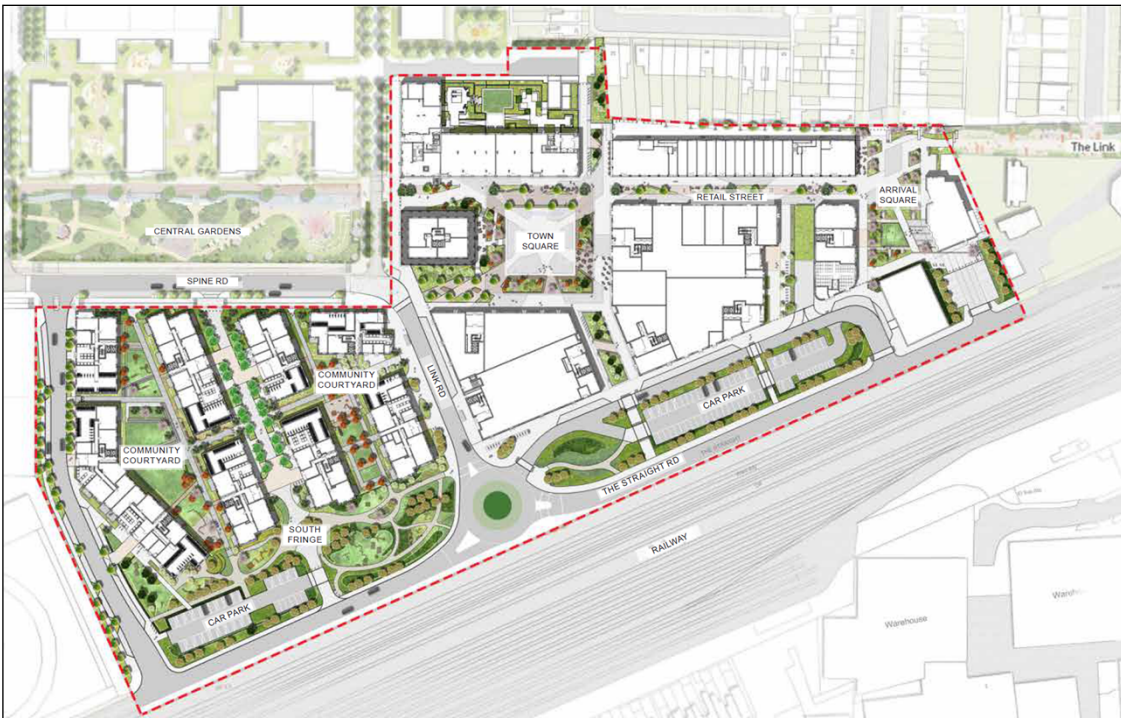








Figure 25: Proposed Landscape Masterplan

Quarter Yard

- Hard landscape with contained 'Greening'
- Multi purpose commercial spaces
- Angular geometry and dynamic design language
- Industrial references in material palette

Parkside Character

- Influenced by parkside location
- Softer character, greater ecological reference
- Less rigid design language / geometry, softer edges, minimal kerbs and definition
- Informal uses of space dominate (such as lawns)

Figure 26: Proposed Landscaping by Character Area

Private Amenity Space & Podium

The private amenity space largely sits in between the proposed buildings and would provide a variety of different spaces for residents to utilise. It is separated into three main courtyards (excluding Building B) as illustrated in Figure 27 below.

This space will provide raised decks, children’s play space, social terraces and seating areas that are wrapped in planting and trees. The applicants landscaping strategy for these areas seeks that ‘Each garden is to impart a unique sense of place and identity – through its scale, design form and style – offering a balance of hard and soft landscapes with integral play space, seating, planters, and small congregation areas’.



Figure 27: Landscaping Access by Area

Public Domain

The public realm would utilise street and pavement paving as well as trees and vegetation to provide a high-quality public realm that is identifiable as public street space.

The public domain would be differentiated between the two character areas. However, in general the following materials:

- Grey Paving;
- Clay or Concrete Paving;
- Stone Paving;
- Concrete slabs;
- Red/Brown Granite;
- Self-binding permeable gravel;
- Timber decking;
- Paving borders; and
- Bronze powder coated planters.

The Town Square would form a key feature of public domain within Phase 2. The town square layout creates a unique amphitheatre for events backed by a ‘green horseshoe’. This layout responds to the retail circuit which wraps around the squares perimeter. On the northern side of the

square an open avenue doubles as restaurant breakout space for summertime outdoor dining overlooking the square. The highlight of this space would be the shard-like run dry mirror pool / play features.



Figure 28: CGI Imagery of Proposed Town Square

Southern Fringe (Infrastructure)

The ‘no build’ zone created by the gas main and associated easement that runs parallel to the Great Western Railway boundary creates a southern fringe to the development upon which car parking and landscaping is proposed. Although the parking areas are a required practical infrastructure for the development, in order to provide wheelchair accessible units in Buildings S and T, the look and feel of the southern fringe as a whole area is certainly more rural and park-like, with resin bounded gravel curvacious pathways and drifts of perennial, shrub and wildflower planting between small copses of trees.

This area of natural landscaping has been enlarged compared to the previous application, with car parking reduced to its absolute minimum. Furthermore, this natural landscape form will allow access to the gas main should it need repair or maintenance investigation.

Play Strategy

The Outline Masterplan set out the level of open space to be provided across The Green Quarter, including a requirement for 2.5 hectares play space. The play strategy within Phase 2 is based upon the anticipated child yield for Phase 2.

This strategy balances formal equipped play provision, aimed at older children, set within adjacent parks (which will be provided by different phases) with doorstep play for under-fives in the courtyards, which will be provided as part of this application.

The quantum of playspace to be provided within Phase 2, relative to the Outline Masterplan requirements, are listed within Table 8 below.

Table 8: Masterplan Site Playspace Quantum

Age Group	Area (Sqm)	Phase 2 Area (Sqm)
0-5 years (Doorstep Play)	3,900 (0.39 Hectares)	5,660sqm
5-11 years/all ages (Local/Neighbourhood Playable Space)	7,600 (0.76 Hectares)	
12+ years (Youth Space)	13,500 (1.35 Hectares)	0
All ages	N/A	1,294sqm
TOTAL	25,000 (2.5 Hectares)	5,954sqm

This play space proposal is well thought-out with a good variety of informal play opportunities for younger children integrated into all the communal greenspaces and play space for juniors in the northern garden which will have public access during daylight hours.

Phase 2 would generate a requirement for 4,142sqm. Phase 2 would provide well in excess of this requirement with 5,954sqm of dedicated play space being provided on site. Additionally, the developer has illustrated (within Figure 29 below) an additional 4,350sqm of play space within the adjacent Central Gardens.

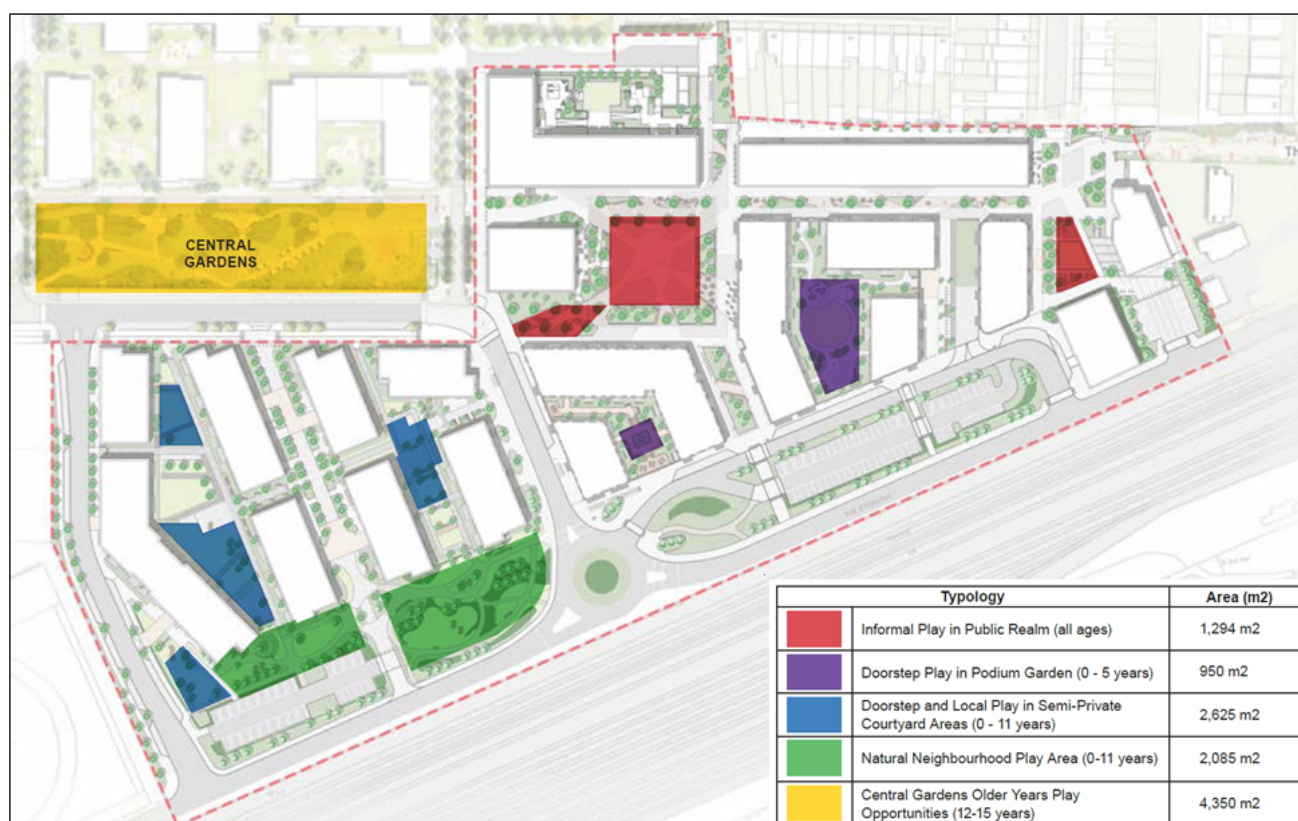


Figure 29: Proposed Playspace across Phase 2

Green Roofs

Whilst there is no set requirement for green roofs as part of the Outline Masterplan, a green roof would be established above all 17 buildings proposed within Phase 2. These would be biodiverse roof tops that would remain inaccessible to residents.

Overall

The proposed landscaping has been well thought out and would provide for a high Urban Greening Factor despite being located within a part of the site designated for residential and commercial development. This would ultimately ensure this area is biodiverse and high in amenity for future

residents in line with the vision for the area.

Impact on amenity of neighbouring properties

Policies D3, D4 of the London Plan (2021), Policy SOU5 of the adopted Ealing Development Sites DPD; Policy 7B of the adopted Ealing Development Management DPD and the NPPF are the key development plan policies used to assess the potential impact of the new buildings on the neighbouring properties along Beaconsfield Road as well as the newly constructed Phase 1 dwellings.

The nearest buildings to existing residential properties would be those that remain approved under the existent Reserved Matters approval (185158REM), being Buildings B, C and D. As these buildings would remain unchanged, the impacts associated with them remain acceptable.

The rest of the buildings remain largely of a similar height to previously assessed and deemed acceptable. They are all below the maximum heights set out within the approved parameter plans.

A sunlight, daylight and overshadowing assessment report has also been submitted in support of the application to assess the potential impact of the development on existing residential properties in Grange Road, Lewis Road and the properties in Phase 1. The assessment demonstrates that there will be no material worsening from the proposal and that in many instances the neighbouring residential occupants would experience a betterment in the amount of daylight and sunlight received.

As such, the scheme would have an acceptable impact on neighbouring properties and accord with the relevant development plan policies used to assess this element of the scheme.

Standard of Accommodation

Policies D2, D3, D4, D6 and D7 of the London Plan (2021), Policies 3.5 and 7B of the adopted Ealing Development Management DPD; Policy SOU5 of the adopted Ealing Development Sites DPD and the NPPF comprise the relevant development plan policies to assess the quality of the proposed residential accommodation.

Internal and external amenity space

Policy D6 of the London Plan (2021); and Ealing Local Variation to London Plan policy 3.5 require that housing developments should be of the highest quality internally and externally. Residential proposals should accord with the Table 3.1 of the London Plan (2021) and the Technical Housing Standards - Nationally Described Space Standards 2015 on internal space, layout, and proportion of wheelchair adaptable units.

The internal and external space of all of the proposed dwellings would comply with the relevant standards, as set out within Table 9 below.

Table 9: Minimum Space Standards

	Adopted minimum standard internal	Adopted standard private amenity space	Proposed minimum internal	Proposed minimum private amenity space
Studio/Suite	37/39sqm	5 sqm	40sqm	5sqm
1B 2P	50sqm	5 sqm	50sqm	5sqm

2B (3/4)	61/70sqm	6sqm/7sqm	61/70sqm	6/7sqm
3B 5P	86sqm	8sqm	87sqm	8sqm
4B 6P	99sqm	9sqm	99sqm	9sqm

There will be a variety of dwelling types within Phase 2 ranging from studios/suites up to four-bedroom family homes. All apartment dwellings will be accessed from a core which provides both lift and stair access to all floors and, where applicable, to the carpark / basement.

The cores will be accessed from the street and also provide access to parking courts/basements and landscaped courtyards. The scheme has full regard to the Mayor’s Housing SPG, maximising dual aspect and east-west facing dwellings whilst minimising single aspect north facing homes.

Accessible Dwellings

The dwellings would also comply with lifetimes homes standards and there would be 150 (12.95%) wheelchair units provided within Phase 2. Additionally, level access would be available from the basement or street to each individual core and lift access, thereby providing accessible access within all buildings.

Overall

On balance, the internal and external design elements of the scheme would accord with the development plan policies and as such would ensure an acceptable level of residential amenity is delivered at this second phase of development.

Affordable Housing provision

Policies GG4, H4, H5, H6 and H7 of the London Plan (2021); Policies 1.2 and 2.8 of the adopted Ealing Development (or Core) Strategy DPD; Policy 3A of the adopted Ealing Development Management DPD; Policy SOU5 of the adopted Ealing Development Sites DPD and the NPPF are the key policies used to assess the affordable housing provision within Phase 2.

The original masterplan scheme ensures that 30% of the residential floorspace across the development would be delivered in the form of affordable housing. The approved Phase 1 has delivered 304 affordable dwellings out of the 623 dwellings. The previous Phase 2 Reserved Matters application sought to deliver 146 affordable dwellings out of 751 dwellings. Phase 3, once built, would deliver 195 affordable dwellings within the 645 total units.

Phase 2 proposes the delivery of 125 affordable rented homes (25 x 1 bed, 66 x 2 beds, 24 x 3 beds, and 10 x 4 beds) and 90 shared ownership homes (23 x 1 bed, 55 x 2 beds, and 12 x 3 beds).

There would be 943 open market homes comprising 28 x studios/suites, 463 x 1 bed, 265 x 2 beds, and 87 x 3 beds. There would be 18.5% affordable housing provision in terms of dwellings which would increase slightly to 18.83% when considered in terms of total floorspace.

Table 10: Unit Type and Tenure

Unit Type	Suite	1B2P	2B3P	2B4P	3B5P	3B6P	4B6P	Total
Private	28	463	167	198	73	14	0	943
Shared Ownership	0	23	23	32	12	0	0	90
Affordable Rent	0	25	12	54	24	0	10	125
Total	28	511	202	284	109	14	10	1158

Figure 29, below, illustrates the affordable housing locations and tenures that have been provided within Phases 1, 2 and 3.

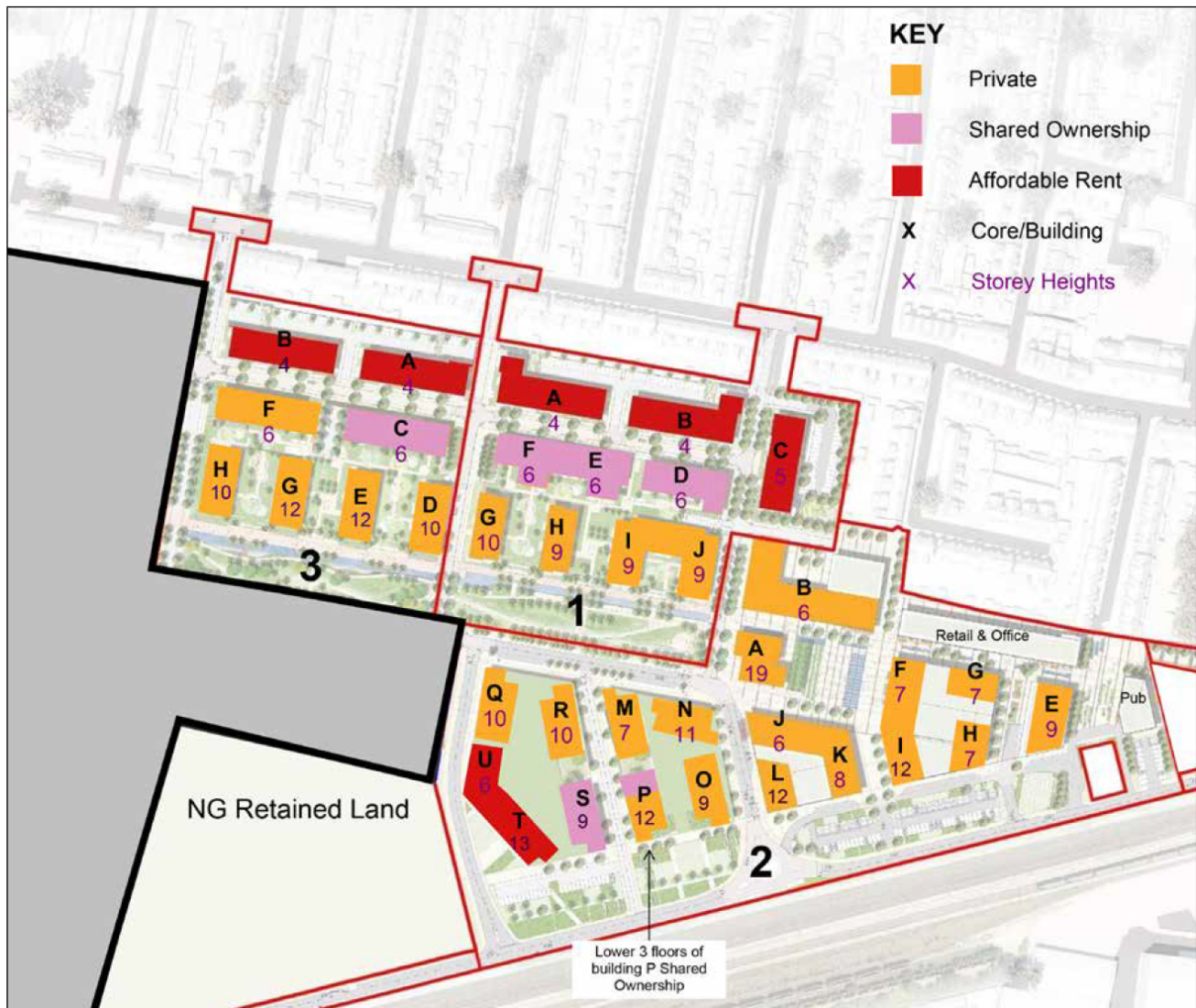


Figure 30: Proposed Affordable Housing Provision within Phases 1, 2 and 3

Table 11 below the running total of the housing that has been approved and is being delivered through Phases 1, 2 and that is proposed within Phase 2. It demonstrates that Phase 2 will provide affordable housing in accordance with the 30% floorspace requirement and the wider development is generally tracking in accordance with the requirement under the S106 agreement.

Table 11: Affordable Housing Running Total

Phase	Dwellings total Sqm GEA/Nos	Private dwellings Sqm GEA/ Nos and %	Total Affordable dwellings Sqm GEA/Nos and %	Shared Ownership Dwelling Nos and %	Social rented dwelling Nos and %	Affordable housing split 50/50 Sqm GEA/Nos
Total Dwellings Provision Sqm GEA	320,000	224,000 (70%)	96,000 (30%)	48,000 (50%)	48,000 (50%)	
Total Dwellings	3,750	2,625 (70%)	1,125 (30%)	563 (50%)	562 (50%)	

Provision Nos.						
Phase 1 Dwellings Sqm GEA	49,610	23,945 (48%)	25,665 (52%)	13,324 (52%)	12,341 (48%)	52/48
Phase 1 Dwellings Nos.	623	319 (51%)	304 (49%)	184 (61%)	120 (39%)	61/39
Phase 2 Dwellings Sqm GEA	88,922	72,465 (81.5%)	16,457 (18.5%)	4,627 (28%)	11,830 (72%)	28/72
Phase 2 Dwellings Nos	1158	943 (81.4%)	215 (18.5%)	90 (42%)	125 (58%)	42/58
Phase 2 Dwellings (Excluded Buildings) Sqm GEA	8,691	8,691 (100%)	0 (0%)	0 (0%)	0 (0%)	0/0
Phase 2 Dwellings (Excluded Buildings) Nos	112	112 (100%)	0 (0%)	0 (0%)	0 (0%)	0/0
Phase 3 Dwellings Sqm GEA	41,038	27,938 (68%)	13,100 (32%)	5,050 (38.5%)	8,050 (61.5%)	39/61
Phase 3 Dwellings Nos.	645	450 (69.8%)	195 (30.2%)	84 (43.0%)	111 (57%)	43/57
Balance Remaining Sqm GEA	131,739	90,961 (69%)	40,778 (31%)	24,999 (61.3%)	15,779 (38.7%)	61/39
Balance Remaining Dwelling Nos.	1,212	801 (66%)	411 (34%)	205 (50%)	206 (50%)	53/47

The agreed maximum rental level for the affordable rented accommodation is that it cannot exceed the lower of 80% of market rent or the Local Housing Allowance for each unit size and this includes all eligible service charges.

Following the early delivery of some 48/52 affordable/open market split at Phase 1, the proposal remains generally on track to deliver 30% of the total floorspace as affordable housing.

Under Schedule 1, Part 7.4(a) of the S106 Agreement dated 29th September 2010, the applicant is obligated to tender the affordable housing provision to Registered Providers (RP) prior to the submission of a Reserved Matters Application unless otherwise agreed with the London Borough of Ealing.

It is noted that Affordable Housing is not a matter strictly for consideration under the Reserved Matters application. Nevertheless, the provision and mix of affordable housing is considered to be acceptable.

Highways, Transport and Parking

Policies T1, T2, T3, T4, T5, T6, T6.1, T6.5 and T.7 of the London Plan (2021), Policies 1.1 and 1.2 of the adopted Ealing Development (or Core) Strategy DPD; Policy 6.13 of the adopted Ealing Development Management DPD; Policy SOU5 of the adopted Ealing Development Sites DPD and the NPPF are the key policies used to assess the highways, transport and parking provision within Phase 2.

The application was accompanied by the following documentation prepared by Transport Planning Practice (TPP):

- Transport Assessment; and
- Outline Car Parking Management Plan.

Trip Generation

A detailed assessment of the impact of trips generated by the wider masterplan has been undertaken as part of the original application as well as the consequent variation application. Given the Phase 2 development forms part of the masterplan site and the quantum of development remains within that approved, the potential impact of the trip generation has been assessed and agreed. Furthermore, it is noted that the alterations to the scheme (when compared with the previous Reserved Matters application) generally result in a positive impact across most modes of travel.

Car Parking

Condition 9 of the Outline Consent (171562VAR) sets out the maximum parking provision across the whole site. For residential parking a maximum of 0.7 spaces per unit across the site is permitted.

A total of 525 residential car parking spaces are proposed for Phase 2 which equates to a parking ratio of 0.35 spaces per unit. Of these, 411 spaces will be provided within the basement beneath Buildings A – K. An additional 28 spaces would be provided at grade in the car park to the south of the buildings.

A total of 82 spaces will be provide at-grade for visitors. These would be in the form of 52 spaces for commercial visitors and 30 spaces for visitors. In addition, 4 car club spaces are proposed. The bulk of these spaces are located in the basement/undercroft located in the centre of Buildings A – L.

A total of 27 Motorcycle Parking spaces are proposed, incorporating 15 spaces within the basement for use by residents as well as 12 at grade spaces for use by residents/visitors/deliveries and/or couriers. Notwithstanding the addition of these space, the proposal remains well within the 0.7 space maximum allowed.

A swept path analysis was carried out to demonstrate that the vehicular access into the car park is acceptable.

A Car Parking Management Plan was prepared by Transport Planning Practice (TPP) and accompanied the planning application. This plan appropriately sets out how the parking provision would be adequately managed, maintained and controlled whilst also minimising any impact on the road network.

Residents of the proposed development would not be eligible for on-street car parking permits.

Transport for London acknowledged that the car parking provision was significantly below the consented maximum level (0.7 spaces per residential unit). TfL did request further reduction and this was undertaken by the applicant, thereby providing additional soft landscaping within the southern edge.

Having regard to the above, the parking provision has been proposed in line with Policy T1 of the London Plan (2021) as well as the Outline Consent. As such, the parking provision is considered to be acceptable.

Electric Vehicle Charging Points

Electric vehicles charging points would be provided in line with the requirements of the London Plan (2021). This requires 20% of bays to have charging facilities with the remaining 80% to incorporate passive infrastructure to allow the future inclusion of charging facilities when required.

Cycle Parking

A total of 2,185 residential cycle spaces as well as 107 visitor cycle spaces are proposed within a number of separate cycle stores spread throughout the wider site. Each building would have access to at least one cycle storage area, either on the ground floor or within the basement.

Through the provision of 2,185 residential cycle spaces, the proposal would be compliant with the cycle parking requirements of the London Plan (2021).

Overall

The proposed street layout is generally consistent with the broad parameters of the consented masterplan scheme approved in October 2017. The parking provision has been minimised in line with the strategic direction of the London Plan (2021) whilst suitable electric vehicle charging points and cycle parking has been provided throughout the scheme. Therefore, the transport, highway and parking elements of the scheme have been assessed and are considered to generally accord with the relevant development plan policies and as such are acceptable.

Environmental Health

Key development plan policies used to assess the environmental impact of the scheme would include Policies SI1, SI7, SI12, SI13 of the London Plan (2021), Policies 1.1, 2.1 and 2.8 of the adopted Ealing Development (or Core) Strategy DPD; Policies 5.21 and 7A of the adopted Ealing Development Management DPD; Policy SOU5 of the adopted Ealing Development Sites DPD and the NPPF.

The outline planning permission includes the relevant conditions to protect the environment and the amenities of existing and future residents with regards to remediation, noise, vibration and air quality. Therefore, the application would accord with the above policies and is considered to be acceptable as regards environmental health matters.

Energy & Sustainability

Policies G1, SI1, SI2, and SI3 of the London Plan (2021), Policy 5.2 of the adopted Ealing Development Management DPD; Policy SOU5 of the adopted Ealing Development Sites DPD and the NPPF are the key policies to assess the energy and sustainability requirements for the development of this scheme.

The proposal is required to comply with Condition 49 of 171562VAR which requires the submission of a Compliance Statement outlining the measures to be incorporated into the development to comply with the relevant Building Regulations and London Plan.

The applicant prepared an Energy and Sustainability Statement (including Circular Economy Statement) prepared by Hoskinson. The proposal has incorporated a range of sustainability features including:

- The development would achieve a minimum 35% reduction in regulated CO2 emissions;
- All dwellings would target a maximum internal daily water consumption of 105 litres/person/day;
- The site incorporates sustainable urban drainage systems;
- The site would benefit from a good existing public transport coverage and sustainable transport modes will be encouraged; and

- Enhancements will be implemented through the provision of open private amenity space, biodiverse roofs and surrounding landscaping.

The Phase 2 application would comply with the approved Energy Strategy by exceeding the required 35% CO2 reduction target in line with the London Plan energy hierarchy.

Councils Energy Consultant reviewed the proposal and confirmed that it was acceptable subject to a carbon offset contribution and additional conditions of consent.

Overall, the proposal has incorporated sustainable design and construction methods to meet the relevant regulations and conditions set out by the Outline Consent (171562VAR).

Section 106 Planning Obligations

The application proposal does not provide any additional development beyond that allowed under the Outline Consent. Additional matters have been identified through the assessment of this Reserved Matters application and, as such, it is recommended that a deed of variation be required for carbon offset contribution and affordable workspace. The specifics of these matters are identified within the Section 106 recommendation above.

Community Infrastructure Levy

The Mayor's Community Infrastructure Levy (CIL) was adopted on 01/04/2012 post the 2010 planning permission. This has introduced a charging system within Ealing of £35 per sqm of gross internal area (GIA) to be paid to the GLA. The total amount of commercial and residential floorspace remains within the thresholds approved under the Outline Consent and does not increase the amount of development on site. As such, there remains no requirement to pay CIL.

Conclusion

This planning application seeks permission for the approval of reserved matters in respect of the Phase 2 residential development within The Green Quarter at Southall as required under condition 2 of planning permission reference 171562VAR dated 16th October 2017. The reserved matters relate specifically to the layout, scale, appearance and landscaping of the residential development.

Phase 2 seeks to deliver 1,158 new homes (market and affordable) as well as 5,533sqm (59,556sq ft) of commercial floorspace across a range of commercial uses. These uses would be delivered across 17 new buildings with associated amenity space, public realm, car, motorcycle and cycle parking.

It is noted that the Reserved Matters have been previously assessed and approved under application 185158REM on the 22nd February 2019. The current application specifically excludes Buildings B, C and D which would retain the existing consent (with works having commenced) for 112 dwellings and approximately 3863sqm of commercial floorspace.

The building scale conforms to the parameters set out in the outline planning permission and drawings provided in the Outline Masterplan application, detailing the maximum and minimum building heights and massing.

The buildings have been revised to respond to comments raised by the Design Review Panel, ensuring that a scheme with a high-quality of architecture that would enhance the local area. The proposed landscaping for the public realm and private amenity space is considered to be of a high standard and would help establish the sense of place.

As such, the key elements of the scheme as they relate to urban design, standard of residential accommodation, impact on neighbouring residential occupiers, affordable housing provision,

highways, transport and parking, environmental health, energy and sustainability, Section 106 and Community Infrastructure Levy matters have had their merits assessed in accordance with the development plan policies, the Outline Masterplan and all other material planning considerations. There have been no objections from third parties.

On balance the proposal would accord with the relevant National, Strategic and Local Planning policies and guidance and approval could reasonably be granted in this case. It is not considered that there are any other material considerations, which would warrant a refusal of the application.

It is therefore recommended that planning permission be **Approved for the reserved matters application for Phase 2 subject to a deed of variation and conditions of consent.**

HUMAN RIGHTS ACT:

In making your decision, you should be aware of and take into account any implications that may arise from the Human Rights Act 1998. Under the Act, it is unlawful for a public authority such as the London Borough of Ealing to act in a manner, which is incompatible with the European Convention on Human Rights.

You are referred specifically to Article 8 (right to respect for private and family life), Article 1 of the First Protocol (protection of property). It is not considered that the recommendation for approval of the grant of permission in this case interferes with local residents' right to respect for their private and family life, home and correspondence, except insofar as it is necessary to protect the rights and freedoms of others (in this case, the rights of the applicant). The Council is also permitted to control the use of property in accordance with the general interest and the recommendation for approval is considered to be a proportionate response to the submitted application based on the considerations set out in this report.

APPENDIX A CONDITIONS AND INFORMATIVES

CONDITIONS:

1. The development permitted shall be begun before the expiration of three years from the date of this permission.

Reason: In order to comply with the provisions of the Town and Country Planning Act 1990 (as amended).

Approved plans

2. The development hereby permitted shall be carried out in accordance with the following approved plans

Site Location Plan (Ref: A725-FOR-00-A-DR-10000 – Rev P2); Existing Site Plan (Ref: A725-FOR-00-A-DR-11000 – Rev P2); Site Plan, Ground Floor Plan (Ref: A725-FOR-00-A-DR-18000 – Rev P9); Site Plan, 1st Floor Plan (Ref: A725-FOR-01-A-DR-18001 – Rev P5); Site Plan, 2nd Floor Plan (Ref: A725-FOR-02-A-DR-18002 – Rev P2); Site Plan, 3rd Floor Plan (Ref: A725-FOR-03-A-DR-18003 – Rev P2); Site Plan, 4th Floor Plan (Ref: A725-FOR-04-A-DR-18004 – Rev P2); Site Plan, 5th Floor Plan (Ref: A725-FOR-05-A-DR-18005 – Rev P2); Site Plan, 6th Floor Plan (Ref: A725-FOR-06-A-DR-18006 – Rev P2); Site Plan, 7th Floor Plan (Ref: A725-FOR-07-A-DR-18007 – Rev P2); Site Plan, 8th Floor Plan (Ref: A725-FOR-08-A-DR-18008 – Rev P2); Site Plan, 9th Floor Plan (Ref: A725-FOR-09-A-DR-18009 – Rev P2); Site Plan, 10th Floor Plan (Ref: A725-FOR-10-A-DR-18010 – Rev P2); Site Plan, 11th Floor Plan (Ref: A725-FOR-11-A-DR-18011 – Rev P2); Site Plan, 12th Floor Plan (Ref: A725-FOR-12-A-DR-18012 – Rev P2); Site Plan, 13th Floor Plan (Ref: A725-FOR-13-A-DR-18013 – Rev P2); Site Plan, 17th Floor Plan (Ref: A725-FOR-17-A-DR-18017 – Rev P2); Site Plan, 18th Floor Plan (Ref: A725-FOR-18-A-DR-18018 – Rev P2); Site Plan, Roof Plan (Ref: A725-FOR-RF-A-DR-18100 – Rev P9); Site Plan, Basement Plan (Ref: A725-FOR-B1-A-DR-18099 – Rev P3); Block A, Ground Floor Plan (Ref: A738-FOR-00-A-DR-12000 – Rev P6); Block A, 1st-16th Floor Plan (Ref: A738-FOR-01-A-DR-12001 – Rev P6); Block A, 17th Floor Plan (Ref: A738-FOR-17-A-DR-12017 – Rev P6); Block A, Roof Plan (Ref: A738-FOR-RF-A-DR-12019 – Rev P2); Block E, Ground Floor Plan (Ref: A742-FOR-00-A-DR-12000 – Rev P7); Block E, 1st, 3rd, 5th, 7th Floor Plan (Ref: A742-FOR-01-A-DR-12001 – Rev P6); Block E, 2nd, 4th, 6th Floor Plan (Ref: A742-FOR-02-A-DR-12002 – Rev P3); Block E, 8th Floor Plan (Ref: A742-FOR-08-A-DR-12008 – Rev P5); Block E, Roof Plan (Ref: A742-FOR-RF-A-DR-12009 – Rev P2); Block F, Ground Floor Plan (Ref: A743-FOR-00-A-DR-12000 – Rev P8); Block F, 1st Floor Plan (Ref: A743-FOR-01-A-DR-12001 – Rev P6); Block F, 2nd-6th Floor Plan (Ref: A743-FOR-02-A-DR-12002 – Rev P5); Block F, Roof Plan (Ref: A743-FOR-RF-A-DR-12007 – Rev P2); Block G, Ground Floor Plan (Ref: A744-FOR-00-A-DR-12000 – Rev P6); Block G, 1st Floor Plan (Ref: A744-FOR-01-A-DR-12001 – Rev P6); Block G, 2nd-6th Floor Plan (Ref: A744-FOR-02-A-DR-12002 – Rev P2); Block G, Roof Plan (Ref: A744-FOR-RF-A-DR-12007 – Rev P2); Block H, Ground Floor Plan (Ref: A765-FOR-00-A-DR-12000 – Rev P6); Block H, 1st Floor Plan (Ref: A765-FOR-01-A-DR-12001 – Rev P6); Block H, 2nd-6th Floor Plan (Ref: A765-FOR-02-A-DR-12002 – Rev P2); Block H, Roof Plan (Ref: A765-FOR-RF-A-DR-12007 – Rev P2); Block I, Ground Floor Plan (Ref: A766-FOR-00-A-DR-12000 – Rev P6); Block I, 1st Floor Plan (Ref: A766-FOR-01-A-DR-12001 – Rev P7); Block I, 2nd to 6th Floor Plan (Ref: A766-FOR-02-A-DR-12002 – Rev P3); Block I, 7th to 9th Floor Plan (Ref: A766-FOR-07-A-DR-12007 – Rev P5); Block I, 10th-11th Floor Plan (Ref: A766-FOR-10-A-DR-12010 – Rev P5); Block I, Roof Plan (Ref: A766-FOR-RF-A-DR-12012 – Rev P2); Block J, K & L, Ground Floor Plan (Ref: A745-FOR-00-A-DR-12000 – Rev P6); Block J & K 1st Floor Plan (Ref: A745-FOR-01-A-DR-12001 – Rev P8); Block J & K 2nd to 5th Floor Plan (Ref: A745-FOR-02-A-DR-12002 – Rev P2); Block J & K, 6th Floor Plan (Ref: A745-FOR-06-A-DR-12006

– Rev P7); Block J & K, 7th Floor Plan (Ref: A745-FOR-07-A-DR-12007 – Rev P6); Block J & K, Roof Plan (Ref: A745-FOR-RF-A-DR-12008 – Rev P2); Block L, 1st Floor Plan (Ref: A767-FOR-01-A-DR-12001 – Rev P5); Block L, 2nd-9th Floor Plan (Ref: A767-FOR-02-A-DR-12002 – Rev P2); Block L, 10th-11th Floor Plan (Ref: A767-FOR-10-A-DR-12010 – Rev P5); Block L, Roof Plan (Ref: A767-FOR-RF-A-DR-12012 – Rev P2); Block M, Ground Floor Plan (Ref: A746-FOR-00-A-DR-12000 – Rev P5); Block M, 01st-06th Floor Plan (Ref: A746-FOR-01-A-DR-12001 – Rev P5); Block M, Roof Plan (Ref: A746-FOR-RF-A-DR-12007 – Rev P2); Block N, Ground Floor Plan (Ref: A757-FOR-00-A-DR-12000 – Rev P5); Block N, 01st, 3rd, 5th, 7th & 9th Floor Plan (Ref: A757-FOR-01-A-DR-12001 – Rev P5); Block N, 2nd, 4th, 6th & 8th Floor Plan (Ref: A757-FOR-02-A-DR-12002 – Rev P3); Block N, 10th Floor Plan (Ref: A757-FOR-10-A-DR-12010 – Rev P5); Block N, Roof Plan (Ref: A757-FOR-RF-A-DR-12012 – Rev P2); Block O, Ground Floor Plan (Ref: A758-FOR-00-A-DR-12000 – Rev P6); Block O, 1st-6th Floor Plan (Ref: A758-FOR-01-A-DR-12001 – Rev P5); Block O, 7th Floor Plan (Ref: A758-FOR-07-A-DR-12007 – Rev P5); Block O, 8th Floor Plan (Ref: A758-FOR-08-A-DR-12008 – Rev P5); Block O, Roof Plan (Ref: A758-FOR-RF-A-DR-12009 – Rev P2); Block P, Ground Floor Plan (Ref: A759-FOR-00-A-DR-12000 – Rev P6); Block P, 1st-6th Floor Plan (Ref: A759-FOR-01-A-DR-12001 – Rev P5); Block P, 7th-9th Floor Plan (Ref: A759-FOR-07-A-DR-12007 – Rev P5); Block P, 10th-11th Floor Plan (Ref: A759-FOR-10-A-DR-12010 – Rev P5); Block P, Roof Plan (Ref: A759-FOR-RF-A-DR-12012 – Rev P2); Block Q, Ground Floor Plan (Ref: A747-FOR-00-A-DR-12000 – Rev P5); Block Q, 1st-9th Floor Plan (Ref: A747-FOR-01-A-DR-12001 – Rev P5); Block Q, 10th Floor Plan (Ref: A747-FOR-10-A-DR-12010 – Rev P5); Block Q, Roof Plan (Ref: A747-FOR-RF-A-DR-12011 – Rev P2); Block R, Ground Floor Plan (Ref: A760-FOR-00-A-DR-12000 – Rev P5); Block R, 1st-6th Floor Plan (Ref: A760-FOR-01-A-DR-12001 – Rev P5); Block R, 7th-9th Floor Plan (Ref: A760-FOR-07-A-DR-12007 – Rev P6); Block R, 10th Floor Plan (Ref: A760-FOR-10-A-DR-12010 – Rev P6); Block R, Roof Plan (Ref: A760-FOR-RF-A-DR-12011 – Rev P2); Block S, Ground Floor Plan (Ref: A762-FOR-00-A-DR-12000 – Rev P6); Block S, 1st-6th Floor Plan (Ref: A762-FOR-01-A-DR-12001 – Rev P5); Block S, 7th Floor Plan (Ref: A762-FOR-07-A-DR-12007 – Rev P5); Block S, 8th Floor Plan (Ref: A762-FOR-08-A-DR-12008 – Rev P5); Block S, Roof Plan (Ref: A762-FOR-RF-A-DR-12009 – Rev P2); Block T & U, Ground Floor Plan (Ref: A748-FOR-00-A-DR-12000 – Rev P6); Block T & U, 1st-5th Floor Plan (Ref: A748-FOR-01-A-DR-12001 – Rev P5); Block T & U, 6th-10th Floor Plan (Ref: A748-FOR-06-A-DR-12006 – Rev P5); Block T & U, 11th-12th Floor Plan (Ref: A748-FOR-11-A-DR-12011 – Rev P5); Block T & U, Roof Plan (Ref: A748-FOR-RF-A-DR-12013 – Rev P2); Block A, Elevation 01 (Ref: A738-FOR-EV-A-DR-20000 – Rev P7); Block A, Elevation 02 (Ref: A738-FOR-EV-A-DR-20001 – Rev P7); Block A, Elevation 03 (Ref: A738-FOR-EV-A-DR-20002 – Rev P7); Block A, Elevation 04 (Ref: A738-FOR-EV-A-DR-20003 – Rev P7); Block E, Elevation 01 (Ref: A742-FOR-EV-A-DR-20000 – Rev P4); Block E, Elevation 02 (Ref: A742-FOR-EV-A-DR-20001 – Rev P4); Block E, Elevation 03 & 04 (Ref: A742-FOR-EV-A-DR-20002 – Rev P4); Block F, Elevation 01 (Ref: A743-FOR-EV-A-DR-20000 – Rev P6); Block F, Elevation 02 (Ref: A743-FOR-EV-A-DR-20001 – Rev P5); Block F, Elevation 03 & 04 (Ref: A743-FOR-EV-A-DR-20002 – Rev P5); Block G, Elevation 01 (Ref: A744-FOR-EV-A-DR-20000 – Rev P4); Block G, Elevation 02 (Ref: A744-FOR-EV-A-DR-20001 – Rev P4); Block G, Elevation 03 & 04 (Ref: A744-FOR-EV-A-DR-20002 – Rev P4); Block H, Elevation 01 (Ref: A765-FOR-EV-A-DR-20000 – Rev P4); Block H, Elevation 02 (Ref: A765-FOR-EV-A-DR-20001 – Rev P4); Block H, Elevation 03 & 04 (Ref: A765-FOR-EV-A-DR-20002 – Rev P4); Block I, Elevation 01 (Ref: A766-FOR-EV-A-DR-20000 – Rev P6); Block I, Elevation 02 (Ref: A766-FOR-EV-A-DR-20001 – Rev P6); Block I, Elevation 03 & 04 (Ref: A766-FOR-EV-A-DR-20002 – Rev P6); Block J, K, Elevation 01 (Ref: A745-FOR-EV-A-DR-20000 – Rev P4); Block J, K, Elevation 02 (Ref: A745-FOR-EV-A-DR-20001 – Rev P4); Block J, K, Elevation 03 (Ref: A745-FOR-EV-A-DR-20002 – Rev P4); Block J, K, Elevation 04 (Ref: A745-FOR-EV-A-DR-20003 – Rev P4); Block J, K, Elevation 05 & 06 (Ref: A745-FOR-EV-A-DR-20004 – Rev P4); Block L, Elevation 01 (Ref: A767-FOR-EV-A-DR-20000 – Rev P6); Block L, Elevation 02 (Ref: A767-FOR-EV-A-DR-20001 – Rev P6); Block L, Elevation 03 & 04 (Ref: A767-FOR-EV-A-DR-20002 – Rev P6); Block M, Elevation 01 (Ref: A746-FOR-EV-A-DR-20000 – Rev P5); Block M,

Elevation 02 (Ref: A746-FOR-EV-A-DR-20001 – Rev P5); Block M, Elevation 03 & 04 (Ref: A746-FOR-EV-A-DR-20002 – Rev P5); Block N, Elevation 01 (Ref: A757-FOR-EV-A-DR-20000 – Rev P4); Block N, Elevation 02 (Ref: A757-FOR-EV-A-DR-20001 – Rev P4); Block N, Elevation 03 & 04 (Ref: A757-FOR-EV-A-DR-20002 – Rev P4); Block O, Elevation 01 (Ref: A758-FOR-EV-A-DR-20000 – Rev P5); Block O, Elevation 02 (Ref: A758-FOR-EV-A-DR-20001 – Rev P5); Block O, Elevation 03 & 04 (Ref: A758-FOR-EV-A-DR-20002 – Rev P5); Block P, Elevation 01 (Ref: A759-FOR-EV-A-DR-20000 – Rev P7); Block P, Elevation 02 (Ref: A759-FOR-EV-A-DR-20001 – Rev P8); Block P, Elevation 03 & 04 (Ref: A759-FOR-EV-A-DR-20002 – Rev P7); Block Q, Elevation 01 (Ref: A747-FOR-EV-A-DR-20000 – Rev P5); Block Q, Elevation 02 (Ref: A747-FOR-EV-A-DR-20001 – Rev P5); Block Q, Elevation 03 & 04 (Ref: A747-FOR-EV-A-DR-20002 – Rev P5); Block R, Elevation 01 (Ref: A760-FOR-EV-A-DR-20000 – Rev P7); Block R, Elevation 02 (Ref: A760-FOR-EV-A-DR-20001 – Rev P7); Block R, Elevation 03 & 04 (Ref: A760-FOR-EV-A-DR-20002 – Rev P7); Block S, Elevation 01 (Ref: A762-FOR-EV-A-DR-20000 – Rev P5); Block S, Elevation 02 (Ref: A762-FOR-EV-A-DR-20001 – Rev P6); Block S, Elevation 03 & 04 (Ref: A762-FOR-EV-A-DR-20002 – Rev P5); Block U, T, Elevation 01 (Ref: A748-FOR-EV-A-DR-20000 – Rev P5); Block U, T, Elevation 02 (Ref: A748-FOR-EV-A-DR-20001 – Rev P5); Block U, T, Elevation 03 (Ref: A748-FOR-EV-A-DR-20002 – Rev P5); Block U, T, Elevation 04 (Ref: A748-FOR-EV-A-DR-20003 – Rev P5); Block U, T, Elevation 05 (Ref: A748-FOR-EV-A-DR-20004 – Rev P6); Block U, T, Elevation 06 & 07 (Ref: A748-FOR-EV-A-DR-20005 – Rev P5); Buildings E, F, G, H, I - Section A-A & B-B (Ref: A725-FOR-SC-A-DR-03201 – Rev P4); Buildings E, F, G, H, I - Section C-C (Ref: A725-FOR-SC-A-DR-03202 – Rev P4); Buildings, A, J, K, L - Section A-A (Ref: A725-FOR-SC-A-DR-03203 – Rev P4); Buildings, A, J, K, L - Section B-B (Ref: A725-FOR-SC-A-DR-03204 – Rev P4); Buildings, M, N, O, P, Q, R, S, T, U - Section A-A & B-B (Ref: A725-FOR-SC-A-DR-03205 – Rev P4); Buildings, M, N, O, P, Q, R, S, T, U - Section C-C & D-D (Ref: A725-FOR-SC-A-DR-03206 – Rev P4); Site Wide Elevation, 01 & 02 (Ref: A725-FOR-SW-A-DR-40000 – Rev P2); Site Wide Elevation, 03, 04, 05 & 06 (Ref: A725-FOR-SW-A-DR-40001 – Rev P2); Site Wide Elevation, 07, 08, 09, & 10 (Ref: A725-FOR-SW-A-DR-40002 – Rev P1); Illustrative Masterplan Phase 2 (Ref: A725-ALD-SW-L-DR-90001 – Rev P3); Landscape Masterplan Phase 2 - Ground Floor Layout (Ref: A725-ALD-GL-L-DR-90011 – Rev P3); Landscape Masterplan Phase 2 - Illustrative Ground (Ref: A725-ALD-SW-L-DR-90002 – Rev P3); Landscape Masterplan Phase 2 - Illustrative Podium (Ref: A725-ALD-SW-L-DR-90003 – Rev P4); Landscape Masterplan Phase 2 - Illustrative Roof (Ref: A725-ALD-SW-L-DR-90004 – Rev P4); Sitewide Sheet Layout (Ref: A725-ALD-SW-L-DR-90015 – Rev P1); General Arrangement – Key (Ref: A725-ALD-GL-L-DR-91010 – Rev P3); General Arrangement - Ground Floor Sheet 1 of 5 (Ref: A725-ALD-GL-L-DR-91011 – Rev P3); General Arrangement - Ground Floor Sheet 2 of 5 (Ref: A725-ALD-GL-L-DR-91012 – Rev P3); General Arrangement - Ground Floor Sheet 3 of 5 (Ref: A725-ALD-GL-L-DR-91013 – Rev P3); General Arrangement - Ground Floor Sheet 4 of 5 (Ref: A725-ALD-GL-L-DR-91014 – Rev P5); General Arrangement - Ground Floor Sheet 5 of 5 (Ref: A725-ALD-GL-L-DR-91015 – Rev P5); General Arrangement - Podium Garden above Block F, G, H (Ref: A725-ALD-01-L-DR-91021 – Rev P2); General Arrangement - Podium Garden above Block J, K, L (Ref: A725-ALD-01-L-DR-91022 – Rev P2); Biodiversity Roofs Planting Strategy (Ref: A725-ALD-RF-L-DR-91041 – Rev P6); Overall Landscape Edging Strategy (Ref: A725-ALD-XX-L-DR-91101 – Rev P4); Overall Landscape Furniture Strategy (Ref: A725-ALD-XX-L-DR-91102 – Rev P4); Overall Landscape Play Strategy (Ref: A725-ALD-XX-L-DR-91361 – Rev P2); Overall Soft Landscape Strategy – Trees (Ref: A725-ALD-XX-L-DR-91401 – Rev P5); Soft Landscape Overall Strategy - Planting Habitats (Ref: A725-ALD-XX-L-DR-91406 – Rev P6); Long section Through Town Square (Ref: A725-ALD-SC-L-DR-93611 – Rev P3); Long section Through Retail Street (Ref: A725-ALD-SC-L-DR-93612 – Rev P2); Long section Through Community Courtyard (Ref: A725-ALD-SC-L-DR-93613 – Rev P1); Long section Through South Fringe and Car Park (Ref: A725-ALD-SC-L-DR-93614 – Rev P1); Long Section Podium Garden Above Block F, G, H (Ref: A725-ALD-SC-L-DR-93615 – Rev P2); Paving Interface Detail Types P11 & P12 (Ref: A725-ALD-GL-L-DR-94111 – Rev P2); Paving Interface Detail Types P12, P14 & P15 (Ref: A725-ALD-GL-L-DR-94113 – Rev P2); Infrastructure Paving Detail Plan Sheet 01 (Ref: A725-ALD-GL-L-DR-94121 – Rev P1); Town Square Paving - Typical Detail And Interface With Water Feature (Ref: A725-ALD-GL-L-DR-94131 – Rev P2); Retail Street Typical Layout (Ref: A725-ALD-GL-L-DR-94132 – Rev P2); Secondary Connecting Street Typical Layout (Ref: A725-ALD-GL-L-DR-94133 –

Rev P1);Town Square Rasied Planter Typical Detail Southeast Side (Ref: A725-ALD-GL-L-DR-94134 – Rev P2); Retail Street Planter Typical Details (Ref: A725-ALD-GL-L-DR-94135 – Rev P1); Parkside Pedestrianised Street Details (Ref: A725-ALD-GL-L-DR-94141 – Rev P1); Parkside Residential Footpath Details (Ref: A725-ALD-GL-L-DR-94142 – Rev P1); Typical Details -Green Roof Landscape Buildup (Ref: A725-ALD-RF-L-DR-94401 – Rev P1); Typical Details - Green & Brown Roof Habitat Enhancement (Ref: A725-ALD-RF-L-DR-94402 – Rev P1); Typical Long Section - Green Roof (Ref: A725-ALD-RF-L-DR-94403 – Rev P1); Typical Long Section - Green & Brown Roof (Ref: A725-ALD-RF-L-DR-94404 – Rev P1); Earthworks Typical Detail: Soil Profiles - On ground (Ref: A725-ALD-GL-L-DR-94405 – Rev P1); Earthworks Typical Detail: Soil Profiles - Over Podium (Ref: A725-ALD-GL-L-DR-94406 – Rev P1); Earthworks Typical Detail Tree Pit Profiles - above ground (Ref: A725-ALD-GL-L-DR-94411 – Rev P1); Earthworks Typical Detail Tree Pit Profiles - above structure within planter (Ref: A725-ALD-GL-L-DR-94412 – Rev P1); Earthworks Typical Detail Tree Pit Profiles - above structure within paving (Ref: A725-ALD-GL-L-DR-94413 – Rev P1)

Reason: For the avoidance of doubt and in the interests of proper planning.

Materials

3. Samples of all external materials shall be submitted to and approved in writing by the local planning authority prior to the commencement of works on site (except for site clearance, demolition and construction of the basements). The development shall be constructed in accordance with the approved materials and be retained as such, thereafter.

Reason: To ensure that the materials harmonise with the surroundings in accordance with policies 1.1(h) and 2.1(c) of the Ealing Development Strategy (2012), policies D3 and D4 of the London Plan (2021) and policy 7B of the Ealing Development Management Development Plan Document 2013.

Details required (refuse, cycle and car parking)

4. Notwithstanding the plans hereby approved no work shall commence (except for site clearance, demolition and construction of the basements) until further details of refuse collection points, secure cycle parking storage, car parking space layout within the basement areas as they relate to safety zones, junction design and perpendicular bays shall be submitted in writing to and approved by the local planning authority, Ealing Council. Such details shall thereafter be implemented in accordance with those approved details and be thereby retained.

Reason: To secure an inclusive, safe and high-quality built environment for all users of the site and be sustainable, and where appropriate constrain local highway impact. In accordance with policies T5, T6, T6.1 and S18 of the London Plan (2021), policies 1.1(e), 1.1(f), 1.1(g) and 1.1(h) of Ealing's adopted Development (or Core) Strategy 2026 (2012); Policies 6.13 and 7.3 of Ealing's Development Management DPD, 2013.

Landscaping

5. The details of hard and soft landscaping including the tree protection and tree planting proposals as submitted to and hereby approved by the local planning authority shall be laid out and planted prior to the occupation of the development. For the avoidance of doubt, details shall include details of the proposed soil, depth, tree pits and their short- and long-term irrigation. Any trees or plants which within 5 years of planting, die, are removed or become seriously damaged or diseased shall be replaced with others of the same size and species and in the same positions within the next planting season. The landscaping shall thereafter be maintained permanently.

Reason: To ensure that the development is landscaped in the interests of the visual character and appearance of the area in accordance with policies G4, G5 and G6 of the London Plan (2021).

6. Details of all boundary walls and/or fences shall be submitted to and approved in writing by the Local Planning Authority shall be approved and in place prior to the first occupation of the development hereby approved/commencement of the use and permanently retained thereafter.

Reason: To ensure a satisfactory and safe development, in accordance with policies 1.1 (e) (g), 1.2(f), 2.1(c) of the Ealing Development (Core) Strategy (2012), policies ELV 7.4 and 7B of the Ealing Development Management Development Plan Document (2013, section 7 of the National Planning Policy Framework (2012) and policy D4 of the London Plan (2021).

7. Prior to the use of the approved development, a 5-year landscape management and maintenance plan, which shall include details of the long term design objectives, management responsibilities, and maintenance schedules for all landscape areas, shall be submitted to and approved in writing by the Local Planning Authority, on a full development or phased basis as appropriate. The approved landscape management and maintenance plan shall be implemented from the completion date of approved development and retained thereafter.

Reason: To ensure that the development is landscaped in the interests of the visual character and appearance of the area in accordance with policies G4, G5 and G6 of the London Plan (2021).

8. Prior to the commencement of the final delivered phase, details of permanent playspace equipment shall be submitted and approved in writing by the Local Planning Authority, covering the full Linear Park in its entirety. Prior to the commencement of the approved development, details of interim playspace equipment installed on earlier phases shall be submitted to and approved in writing by the Local Planning Authority, on a phased basis. The approved playspace equipment shall be maintained thereafter on an interim or permanent basis as appropriate and shall be installed prior to completion on a phased or full Park basis and retained thereafter.

Reason: To ensure that the development is in accordance with policy S4 of the London Plan (2021).

9. Details of the green roof construction and specifications shall be submitted to and approved in writing by the Local Planning Authority prior to the commencement of works. The green roof shall be:
- a) Biodiversity based with extensive substrate base (depth 50-150mm);
 - b) Laid out in accordance with drawing A725-HTA-C0-PS-A-DR-91000 (Revision P7; dated 11/12/2020) hereby approved;
 - c) Planted / seeded with an agreed mix of species within the first planting season following the practical completion of the building works for that phase;
 - d) Efficiently and effectively drained and irrigated to sustain the relevant vegetation.

The green roofs shall not be used as amenity or sitting out space of any kind and shall be maintained as such thereafter and no change therefrom shall take place without the prior written consent of the Local Planning Authority.

Reason: To ensure that the development is landscaped in the interests of the visual character and appearance of the area in accordance with policies G4, G5 and G6 of the London Plan (2021).

Low NOx Boiler specification

10. Notwithstanding the plans hereby approved, details shall be submitted to the Local Planning Authority for approval before Phase 2 of the development is commenced, (except for site clearance, demolition and construction of the basements) for the installation of Ultra Low NOx communal gas boilers with maximum NOx emissions of under 0.040 g/kWh. The works, as approved, shall be completed prior to occupation and thereafter permanently retained.

Reason: To minimise the impact of building emissions on local air quality in the interests of health, in accordance with policy SI1 of the London Plan (2021), the Mayor's Sustainable Design and Construction SPG; policies 1.1(e) and (j) of Ealing's Development (or Core) Strategy 2012, and policy 7A of Ealing's Development Management DPD.

Details required Sound Insulation

11. Notwithstanding the plans hereby approved further details of the flat layouts shall be submitted to and approved in writing by the local planning authority prior to the completion of the super structure of the first block to be constructed. Such details shall demonstrate an enhanced sound insulation value $D_{nT,w}$ and $L'_{nT,w}$ of at least 5dB above the Building regulations value, for the floor/ceiling/wall structures separating different types of rooms/uses in adjoining dwellings and communal uses, for example, living/dining room and kitchen above/below/adjoining bedroom of separate dwelling/bedrooms adjoining communal rooms/stairs/lifts etc. The approved details shall be fully implemented prior to first occupation of any of the residential dwellings and be permanently retained thereafter.

Reason: To safeguard the amenities of the occupiers of the residential accommodation within the development, in accordance with policies 1.1 and 1.2 of the Ealing Development (Core) Strategy (2012), policies 7A & 7B of the Ealing Development Management Development Plan Document (2013), policies D6 and D14 of the London Plan (2021), Ealing's SPG10 and the National Planning Policy Framework (2021).

12. Prior to commencement of the superstructure of each building within Phase 2 of the development hereby approved, a noise assessment shall be submitted to the Council for approval in writing, of external noise levels emitted from the Cadent PRS, transport and commercial/ cultural sources outside any Covid restrictions or holiday periods, having regard to the assessment standards of the Council's SPG10. Details shall include the sound insulation of the building envelope including glazing specifications (laboratory tested including frames, seals and any integral ventilators, approved in accordance with BS EN ISO 10140-2:2010) and of acoustically attenuated mechanical ventilation and cooling as necessary (with air intake from the cleanest aspect of the building and details of self-noise) to achieve internal noise limits specified in SPG10. Best practicable mitigation measures shall also be implemented, as necessary, in external amenity spaces to achieve criteria of BS8233:2014. The approved details shall be implemented prior to occupation of the development and thereafter be permanently retained.

Reason: To safeguard the amenities of the occupiers of the residential accommodation within the development from the noise generated by the adjoining Cadent Gas land, or any other sources of noise, in accordance with policies 1.1 and 1.2 of the Ealing Development (Core) Strategy (2012), policies 7A & 7B of the Ealing Development Management Development Plan Document (2013), policies D6 and D14 of the London Plan (2021), Ealing's SPG10 and the National Planning Policy Framework (2021).

Energy

- 13.
- a) Prior to construction completion and occupation, the permitted development shall implement and maintain, and in the case of energy generation equipment confirm as operational, the approved measures to achieve an overall sitewide reduction in regulated CO₂ emissions

against SAP10 standards of at least 52.94% (equating to 637.4 tonnes of CO₂ per year) beyond Building Regulations Part L 2013. These CO₂ savings shall be achieved through the Lean, Clean, Green Energy Hierarchy as detailed in the Energy Statement prepared by Hodkinson in September 2021, and in their addendum prepared in February 2022, including:

- i. Lean, passive design measures to achieve an annual reduction of at least 12.67% equating to at least 140.2 tonnes in regulated carbon dioxide (CO₂) emissions over BR Part L 2013 for the residential development, and at least 37.38%, equating to at least 36.3 tonnes, over Part L 2013 for the non-residential space.
 - ii. Clean, energy generation systems from connection to the Green Quarter District Heat Network to achieve an annual reduction of at least 36%, equating to 434 tonnes, in regulated carbon dioxide (CO₂) emissions over Part L 2013. The Air Source Heat Pumps to provide 80% of heat demand.
 - iii. Green, renewable energy equipment including the incorporation of photovoltaic panels with a combined total capacity of (approx) 140 kWp to achieve an annual reduction of at least 2.20%, equating to 26.5 tonnes, in regulated carbon dioxide (CO₂) emissions over Part L 2013.
 - iv. Seen, heat and electric meters installed to monitor the performance of the PV and the carbon efficiency (SCOP) of the heat pumps including the heat generation and the combined parasitic loads of the heat pumps, and the flow and return Delta T of the DHN supply to the phase 2 buildings.
- b) Prior Installation, details of the proposed renewable/low-carbon energy equipment, and associated monitoring devices required to identify their performance, shall be submitted to the Council for approval. The details shall include the DHN schematics, the exact number of heat pump collectors, the heat pump thermal kilowatt output, heat output pipe diameter(s), parasitic load supply schematics, monthly energy demand profile, and the exact number of PV arrays, the kWp capacity of each array, the orientation, pitch and mounting of the panels, and the make and model of the panels. The name and contact details of the LZC installation contractor(s), and if different, the commissioning electrical or plumbing contractor, should be submitted to the Council prior to installation.
 - c) On completion of the installation of the LZC equipment copies of the MCS certificates and all relevant commissioning documentation shall be submitted to the Council.
 - d) The development shall incorporate the overheating and cooling measures detailed in the Dynamic Overheating Report submitted by Hodkinson September 2021 (version-3). The assessment shall be compliant with the relevant CIBSE guidance TM49 and/or TM59 and also modelled against the DSY1 (average summer) weather data files, and the more extreme weather DSY2 (2003) and DYS3 (1976) files.
 - e) All boilers to serve the energy requirements of the development detailed in the approved energy strategy should be specified with NO_x emissions (g/m²) that are compliant with or better than the ultra-low NO_x (g/m²) benchmarks as set out at Appendix 5 of the Mayor's Sustainable Design and Construction SPG.
 - f) Details of the layout of the energy plant room and equipment and the method of how the facility shall be designed to connect to, or allow for future connection to, an offsite district heating network, if an on-site energy centre is provided;
 - g) Within three months of the occupation/first-use of the development a two-page summary report prepared by a professionally accredited person comparing the "as built stage" TER to BER/DER figures against those in the final energy strategy along with the relevant Energy Performance Certificate(s) (EPC) and/or the Display Energy Certificate(s) (DEC's) shall be submitted to the Council for approval.

Reason: In the interest of addressing climate change and to secure environmentally sustainable development in accordance with policies SI2 and SI3 of the London Plan (2021), and the relevant guidance notes in the GLA Energy Assessment Guidance 2020, policies LV5.2 and 7A of Ealing's Development Management DPD 2013, and policies 1.1(k) and 1.2(f) of Ealing's Development (Core) Strategy 2012.

14. In order to implement Ealing Council DPD policy E5.2.3 (post-construction energy equipment monitoring), and key parts of London Plan policy SI2 (“be Seen”), the developer shall:
- a) Enter into a legal agreement with the Council to secure a S106 financial contribution for the post-construction monitoring of the renewable/low carbon technologies to be incorporated into the development and/or the energy use of the development as per energy and CO₂ Condition(s).
 - b) Upon final construction of the development, or relevant phases of the development, the agreed suitable devices for monitoring the District Heat Network (for Phase 2) , the performance/efficiency (SCOP) of any renewable/low-carbon energy equipment shall be installed. The monitored data shall be automatically submitted to the Council at daily intervals for a period of four years from occupation and full operation of the energy equipment. The installation of the monitoring devices and the submission and format of the data shall be carried out in accordance with the Council's approved specifications as indicated in the Automated Energy Monitoring Platform (AEMP) information document. The developer must contact the Council's chosen AEMP supplier (Emergence Ltd) on commencement of construction to facilitate the monitoring process.
 - c) Upon final completion of the development and prior to occupation, the developer must submit to the Council proof of a contractual arrangement with a certified contractor that provides for the ongoing, commissioning, maintenance, and repair of the renewable/low-carbon energy equipment for a period of four years from the point that the building is occupied and the equipment fully operational.

Reason: To monitor the effectiveness and continued operation of the renewable/low carbon energy equipment in order to confirm compliance with energy policies and establish an in-situ evidence base on the performance of such equipment in accordance with London Plan (2021) policy SI2 (“Be Seen” stage of the energy hierarchy), Ealing's Development (Core) Strategy 2026 (3rd April 2012) and Development Management DPD policy 5.2, E5.2.3, and Policy 2.5.36 (Best Practice) of the Mayor's Sustainable Design & Construction SPG.

15. In order to demonstrate compliance with the ‘be seen’ post-construction monitoring requirement of Policy SI 2 of the London Plan, the legal Owner shall at all times and all in all respects comply with the energy monitoring requirements set out in points a, b and c below. In the case of non-compliance the legal Owner shall upon written notice from the Local Planning Authority immediately take all steps reasonably required to remedy non-compliance.

Within four weeks of planning permission being issued by the Local Planning Authority, the Owner is required to submit to the GLA accurate and verified estimates of the ‘be seen’ energy performance indicators, as outlined in Chapter 3 ‘Planning stage’ of the GLA ‘Be seen’ energy monitoring guidance document, for the consented development. This should be submitted to the GLA's monitoring portal in accordance with the ‘Be seen’ energy monitoring guidance.

Once the as-built design has been completed (upon commencement of RIBA Stage 6) and prior to the building(s) being occupied (or handed over to a new legal owner, if applicable), the legal Owner is required to provide updated accurate and verified estimates of the ‘be seen’ energy performance indicators for each reportable unit of the development, as per the methodology outlined in Chapter 4 ‘As-built stage’ of the GLA ‘Be seen’ energy monitoring guidance. All data and supporting evidence should be uploaded to the GLA's monitoring portal. In consultation with the Council's chosen Automated Energy Monitoring Platform provider the owner should also confirm that suitable monitoring devices have been installed and maintained for the monitoring of the in-use energy performance indicators, as outlined in Chapter 5 ‘In-use stage’ of the GLA ‘Be seen’ energy monitoring guidance document.

Upon completion of the first year of occupation following the end of the defects liability period (DLP) and for the following four years, the legal Owner is required to provide accurate and verified annual in-use energy performance data for all relevant indicators under each reportable

unit of the development as per the methodology outlined in Chapter 5 'In-use stage' of the GLA 'Be seen' energy monitoring guidance document. All data and supporting evidence should be uploaded to the GLA's monitoring portal. This condition will be satisfied after the legal Owner has reported on all relevant indicators included in Chapter 5 'In-use stage' of the GLA 'Be Seen' energy monitoring guidance document for at least five years.

In the event that the in-use evidence submitted shows that the as-built performance estimates have not been or are not being met, the legal Owner should use reasonable endeavours to investigate and identify the causes of underperformance and the potential mitigation measures and set these out in the relevant comment box of the 'be seen' spreadsheet. Where measures are identified, which it would be reasonably practicable to implement, an action plan comprising such measures should be prepared and agreed with the Local Planning Authority. The measures approved by the Local Planning Authority should be implemented by the legal Owner as soon as reasonably practicable.

Reason: In order to ensure that actual operational energy performance is minimised and demonstrate compliance with the 'be seen' post-construction monitoring requirement of Policy SI 2 of the London Plan

16.

- a) The non-residential element of the development shall be registered with Building Research Establishment (BRE) and achieve BREEAM Rating of "Excellent" (76.27%) for the shell-and-core Office space, and "Very Good (59.70%) for the Retail space.
- b) Within 3 months of each non-residential element of the development, Interim BREEAM NC Assessment and related Certification verified by the BRE shall be submitted to the Local Planning Authority for written approval.
- c) Within 3 months from the date of first occupation of each non-residential element of the development, BREEAM 'Post Construction Stage' Assessment and related Certification verified by the BRE should be submitted to the Local Planning Authority for written approval confirming the BREEAM standard and measures have been implemented.
- d) Following any approval of a 'Post Construction Stage' assessment and certificate of the development, the approved measures and technologies to achieve the BREEAM Very Good or higher standard shall be retained in working order in perpetuity.

Reason: In the interest of addressing climate change and to secure sustainable development in accordance with policies Si2 and Si3 of the London Plan (2021), and the relevant guidance notes in the GLA Energy Assessment Guidance 2020, policies LV5.2 and 7A of Ealing's Development Management DPD 2013, and policies 1.1(k) and 1.2(f) of Ealing's Development (Core) Strategy 2012, policies LV5.2 and 7A of the Ealing Development Management DPD 2013, and Policies 1.1(k) and 1.2(f) of the Ealing Development (Core) Strategy 2012.

17. Once the as-built design has been completed (upon commencement of RIBA Stage 6) and prior to the building(s) being occupied (or handed over to a new owner, if applicable), the legal owner(s) of the development should submit the post-construction Whole Life-Cycle Carbon (WLC) Assessment to the GLA at: ZeroCarbonPlanning@london.gov.uk.

The owner should use the post construction tab of the GLA's WLC assessment template and this should be completed accurately and in its entirety, in line with the criteria set out in the GLA's WLC Assessment Guidance. The post-construction assessment should provide an update of the information submitted at planning submission stage (RIBA Stage 2/3), including the WLC carbon emission figures for all life-cycle modules based on the actual materials, products and systems used. The assessment should be submitted along with any supporting evidence as per the guidance and should be received three months post as-built design completion, unless otherwise agreed.

Modules A1-A5 (Sourcing / Construction) should achieve an assessment of 633 KgCO₂e/m² and B1-C4 (In-use / End of life) 418 KgCO₂e/m² as detailed in the WLC Assessment (version-5) prepared by Hodkinson in September 2021, with a total carbon emissions baseline scenario (over 60 years) of 1,051 KgCO₂e/m² (64,046 t/CO₂).

Reason: To ensure whole life-cycle carbon is calculated and reduced and to demonstrate compliance with Policy SI2(F) of the London Plan.

INFORMATIVES:

1. The decision to grant planning permission has been taken having regard to the policies and proposals in the Ealing Development (Core) Strategy 2012, the Ealing Development Management Development Plan Document 2013, the London Plan (2021), the National Planning Policy Framework 2021 and to all relevant material considerations including Supplementary Planning Guidance:

National Planning Policy Framework (2021)

1. Introduction
 2. Achieving sustainable development
 3. Plan-making
 4. Decision-making
 5. Delivering a sufficient supply of homes
 6. Building a strong, competitive economy
 7. Ensuring the vitality of town centres
 8. Promoting healthy and safe communities
 9. Promoting sustainable transport
 11. Making effective use of land
 12. Achieving well-designed places
 14. Meeting the challenge of climate change, flooding and coastal change;
 15. Conserving and enhancing the natural environment
 16. Conserving and enhancing the historic environment
- Annex 1: Implementation
Annex 2: Glossary

London Plan (2021)

- GG1 – Building strong and inclusive communities
- GG2 – Making the best use of land
- GG3 – Creating a healthy city
- GG4 – Delivering the homes Londoners need
- GG5 – Growing a good economy
- GG6 – Increasing efficiency and resilience
- SD1 – Opportunity Areas
- SD6 – Town centres and high streets
- SD10 – Strategic and local regeneration
- D1 – London’s form, character and capacity for growth
- D2 – Infrastructure requirements for sustainable densities
- D3 – Optimising site capacity through the design-led approach
- D4 – Delivering good design
- D5 – Inclusive design
- D6 – Housing quality and standards
- D7 – Accessible housing
- D8 – Public Realm
- D9 – Tall buildings

D10 – Basement development
D11 – Safety, security and resilience to emergency
D12 – Fire safety
D13 – Agent of change
D14 – Noise
H1 – Increasing Housing Supply
H5 – Threshold Approach to Applications
H6 – Affordable Housing Tenure
H7 – Monitoring of affordable housing
H10 – Housing Size Mix
S4 – Play and Informal Recreation
G1 – Green Infrastructure
G4 – Local green and open space
G5 – Urban greening
G6 – Biodiversity and access to nature
G7 – Trees and woodlands
G9 - Geodiversity
S11 – Improving air quality
S12 – Minimising greenhouse gas emissions
S13 – Energy infrastructure
S14 – Managing heat risk
S15 – Water infrastructure
S16 - Digital connectivity infrastructure
S17 – Reducing waste and supporting the circular economy
S112 – Flood Risk Management
S113 – Sustainable drainage
S114 – Waterways - Strategic Role
S116 – Waterways, Use and Enjoyment
S117 – Protecting and enhancing London’s waterways
T1 – Strategic approach to transport
T2 – Healthy Streets
T3 – Transport capacity, connectivity and safeguarding
T4 – Assessing and mitigating transport impacts
T5 – Cycling
T6 – Car parking
T6.1 – Residential parking
T6.5 – Non-residential disabled persons parking
T7 – Deliveries, servicing and construction
T9 – Funding transport infrastructure through planning
DF1 Delivery of the Plan and Planning Obligations
M1 – Monitoring

Mayor of London Supplementary Planning Guidance

Housing (March 2016)
Use of planning obligations in the funding of Crossrail and the Mayoral CIL (April 2013)
Social Infrastructure (May 2015)
The Control of Dust and Emissions during Construction and Demolition (July 2014)
Accessible London: Achieving an Inclusive Environment (October 2014)
Affordable Housing and Viability (August 2017)

Ealing Development (Core) Strategy 2026 Development Plan Document, 2012

Chapter 1 - Vision for Ealing 2026
1.1 Spatial Vision for Ealing (a), (b), (c), (d), (e), (f), (g), (h), (i), (j), (k)

1.2 Delivery of the Vision for Ealing 2026 (a), (c), (d), (e), (f), (h), (i), (k), (m)
Chapter 2 - Development in the Uxbridge Road/ Crossrail Corridor
2.1 Realising the Potential of the Uxbridge Road/Crossrail Corridor (a), (b), (c), (d)
2.8 Revitalise Southall Town Centre
2.10 Residential Neighbourhoods
Chapter 5 – Protecting and enhancing Ealing’s Green and Open Spaces
5.3 Protect & Enhance Green Corridors (a)
5.4 Protect the Natural Environment – Biodiversity and Geodiversity (a), (c)
5.5 Promoting Parks, Local Green Space and Addressing Deficiency (b)
5.6 Outdoor Sports and Active Recreation
Chapter 6 – Ensuring Sustainable Delivery
6.1 Physical Infrastructure
6.2 Social Infrastructure
6.3 Green Infrastructure
6.4 Planning Obligations and Legal Agreements

Development Management Development Plan Document, 2013

Ealing Local Variation to London Plan Policy 2.18 Green Infrastructure: The Network of Open and Green Spaces
Ealing Local Variation to London Plan Policy 3.4 Optimising Housing Potential
Ealing Local Variation to London Plan Policy 3.5 Quality and Design of Housing Developments
Ealing Local Policy - 3A Affordable Housing
Ealing Local Policy 4A – Employment Uses
Ealing Local Variation to London Plan Policy 4.5 London’s Visitor Infrastructure
Ealing Local Policy 4B Retail
Ealing Local Variation to London Plan Policy 5.2 Minimising Carbon Dioxide Emissions
Ealing Local Variation to London Plan Policy 5.10 Urban Greening
Ealing Local Variation to London Plan Policy 5.12 Flood Risk Management
Ealing Local Variation to London Plan Policy 5.21 Contaminated Land
Ealing Local Variation to London Plan Policy 6.13 Parking
Ealing Local Policy 7A Amenity
Ealing Local Variation to London Plan Policy 7.3 Designing Out Crime
Ealing Local Variation to London Plan Policy 7.4 Local Character
Ealing Local Policy 7B Design Amenity
Ealing Local Variation to London Plan Policy 7.7 Location and Design of Tall and Large Buildings
Ealing Local Policy - 7D Open Space
EA Ealing Local Policy Presumption in Favour of Sustainable Development

Development Sites Development Plan Document, 2013

SOU5 Southall West
Southall Opportunity Area Planning Framework SPD (July 2014)

Ealing Informal Planning Documents/Guidance

SPG 3: Air Quality
SPG 4: Refuse and Recycling
SPG 9: Trees
SPD9: Legal Agreements, Planning Obligations and Planning Gain

In reaching this decision, specific consideration was given to whether the subject site is satisfactory for the proposed development, given its location, the impact on the amenities of occupiers of neighbouring residential properties and the character and appearance of the local area. All other considerations include affordable housing, housing mix, commercial uses, extension of Southall town centre, internal and external design matters, air quality, noise, sustainability, transport,

community infrastructure levy and Section 106 planning obligations. Consideration was also given to consultation comments and relevant policies. It is considered, given the location together with the nature of the proposed uses and form of development, that planning permission could reasonably be granted in this case. It is not considered that there are any other material considerations, which would warrant a refusal of the application.

2. Demolition and construction works, audible beyond the boundary of the site shall only be carried on between the hours of 0800 - 1800hrs Mondays to Fridays and 0800 - 1300hrs on Saturdays and at no other times, including Sundays and Bank Holidays. No bonfires shall be lit on site. BPM & mitigation measures can be found in the following guidance:
 - i. 'Guidance on the Assessment of dust from demolition and construction', IAQM, February 2014
 - ii. 'The control of dust and emissions from Construction and Demolition' Draft SPG, GLA, 2013
 - iii. BS 5228-1:2009 - Code of practice for noise & vibration control on construction & open sites-Part 1: Noise

3. Prior to the commencement of any site works, all sensitive properties surrounding the site boundary shall be notified in writing of the nature and duration of works to be undertaken, and the name and address of a responsible person, to whom an enquiry/complaint should be directed. A minimum written period of 1 month would be required.

4. Dust mitigation and control of exhaust emissions from construction vehicles should comply with the Mayor's (GLA and London Councils) 'Best Practice Guidance' to control dust and emissions from construction.

5. The developer will be liable for the cost of any repairs to damage to the footway directly resulting from the construction work. It is recommended that a footway/carriage way condition survey is carried out prior to the start of construction work, in conjunction with the Highways Section.

6. Ground Investigation:
 - a) Reference should be made at all stages to appropriate current guidance and codes of practice this would include:
 - The report of the findings must include:
A timetable of works and site management procedures.
 - (i) a survey of the extent, scale and nature of contamination;
 - (ii) an assessment of the potential risks to:
 - human health,
 - property (existing or proposed) including buildings, crops, livestock, pets, woodland and service lines and pipes,
 - adjoining land,
 - groundwater and surface waters,
 - ecological systems,
 - archaeological sites and ancient monuments
 - Model Procedures for the Management of Land Contamination, CLR 11, Environment Agency, 2004
 - Updated technical background to the CLEA model, Science Report: SC050021/SR3, Environment Agency, 2009
 - LQM/CIEH Generic Assessment criteria for Human Health Risk Assessment (2nd Edition), 2009
 - BS10175:2011 Investigation of potentially contaminated sites – Code of Practice
 - Secondary Model Procedure for the Development of Appropriate Soil Sampling Strategies for Land Contamination; Environment Agency, 2001

- Verification of Remediation of Land Contamination', Report: SC030114/R1, Environment Agency, 2010
- National Planning Policy Framework (Paragraph 109, 120, 121);
- Guidance for the safe development of housing on land affected by contamination, NHBC & Environment Agency, 2008

b) Clear site maps should be included in the reports showing previous and future layouts of the site, potential sources of contamination, the locations of all sampling points, the pattern of contamination on site, and to illustrate the remediation strategy.

c) All raw data should be provided in a form that can be easily audited and assessed by the council.(e.g. trial pit logs and complete laboratory analysis reports)

d) on-site monitoring for ground gases with any relevant laboratory gas analysis;

'Good practice on the testing and verification of protection systems for buildings against hazardous ground gases, (C735), CIRIA, August 2014

e) Details as to reasoning, how conclusions were arrived at and an explanation of the decisions made must be included. (e.g. the reasons for the choice of sampling locations and depths).

7. Noise

a) SPG10 requires that acoustic measurements are carried out and that precise calculations are made for the building envelope insulation. In calculating the minimum sound reductions the following is required:

- A precise sound insulation calculation under the method given at BS EN12354-3: 2000, for the various building envelopes, using the worst case one hour data (octave band linear noise spectra from 63 Hz - 4k Hz) by night and day, to arrive at the minimum sound reductions necessary to meet the SPG10 internal data.
- Approved laboratory sound insulation test certificates for the chosen windows, including frames and seals and also for ventilators, in accordance with BS EN ISO 140-3: 1995 & BS EN ISO 10140-2:2010, to verify the minimum sound reductions calculated.

Compliance with the internal and external criteria set at SPG10

8. Ground Water Discharge: With regard to surface water drainage it is the responsibility of a developer to make proper provision for drainage to ground, water courses or a suitable sewer. In respect of surface water, it is recommended that the applicant should ensure that storm flows are attenuated or regulated into the receiving public network through on or off-site storage. When it is proposed to connect to a combined public sewer, the site drainage should be separate and combined at the final manhole nearest the boundary. Connections are not permitted for the removal of groundwater. Where the developer proposes to discharge to a public sewer, prior approval from Thames Water Developer Services will be required. They can be contacted on 0845 850 2777.

9. A ground water risk management permit from Thames Water will be required for discharging ground water into a public sewer. Any discharge made without a permit is deemed illegal and may result in prosecution under the provisions of the Water Industry Act 1991. Permit enquiries should be directed to Thames Water's Risk Management Team by telephoning 020 3577 9483 or by emailing www.riskmanagement@thameswater.co.uk. Application forms should be completed on line via www.thameswater.co.uk/wastewaterquality.

10. Legal changes under the Water Industry (Scheme for the adoption of private sewers) Regulations 2011 mean that the sections of pipes you share with neighbours or are situated outside of your property boundary which connect to a public sewer are likely to have transferred to Thames Water's ownership. Should your proposed building work fall within 3 metres of these pipes we recommend that you contact Thames Water to discuss their status in more detail and

to determine if a building over/near to agreement is required. You can contact Thames Water on 0800 009 3921 or for more information please visit the Thames Water website at www.thameswater.co.uk

11. The developer is advised that should any external plant be installed the rating noise level emitted from the proposed external plant and machinery at the proposed development, as assessed under BS4142: 1997, shall be lower than the existing background noise level by at least 5 dBA as measured at 3.5 m from the nearest ground floor sensitive facade and 1m from upper floor noise sensitive facades, during the relevant periods of operation.
12. Under the terms of the Water Resources Act 1991, and the Thames Region Land Drainage Byelaws 1981, the prior written consent of the Environment Agency is required for any proposed works or structures, in, under, over or within 8 metres of the top of the bank of the Yeading Brook, designated a 'main river'.
13. Under the terms of the Land Drainage Act 1991, the prior written consent of the Environment Agency is required for proposed works or structures which will affect the flow of the, designated an 'ordinary watercourse'.
14. The Thames Region (which includes all the London Boroughs) has been identified as an area of 'serious water stress' under our published document 'identifying Areas of Water Stress'.
15. All landscaping areas (including private gardens) should be planted with drought tolerant plants, trees and grasses to minimise water requirements.
16. Roof water down pipes should be connected to the drainage system either directly, or by means of back inlet gullies provided with sealing plates and drainage from covered car parking floors should pass through an approved oil separator before connecting to the surface water system.
17. The development of this site is likely to damage archaeological remains. The applicant should therefore submit detailed proposals in the form of an archaeological project design. This design should be in accordance with the appropriate English Heritage guidelines.
18. Prior to commencement of development of construction and demolition works, details of noise/vibration and dust mitigation measures shall be submitted to the Environmental Health section for approval.
19. Any stone crushing plant proposed on site during the demolition process may require authorisation under the Environmental Protection Act (Prescribed Processes and Substances) Regulations 1991, SI 472. Contact the IBE Environmental Health Section on 0208 825 8111.
20. The applicant/developer should refer to the current "Code of Practice for Works affecting the Canal & River Trust" to ensure that any necessary consents are obtained. Please visit <https://canalrivertrust.org.uk/business-and-trade/undertaking-works-on-our-property-and-our-code-of-practice>.
21. The applicant is advised that surface water discharge to the waterway will require prior consent from the Canal & River Trust. Please contact Claire McLean, Area Planner - London from the Canal & River Trust on 0203 204 4409 or at Claire.mclean@canalrivertrust.org.uk